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June 1, 2022

British Columbia Utilities Commission  
Suite 410, 900 Howe Street  
Vancouver, BC  
V6Z 2N3

Attention: Mr. Patrick Wruck, Commission Secretary

Dear Mr. Wruck:

**Re: FortisBC Energy Inc. – Mainland and Vancouver Island Service Area  
Commodity Cost Reconciliation Account (CCRA) and Midstream Cost  
Reconciliation Account (MCRA) Quarterly Gas Costs  
2022 Second Quarter Gas Cost Report**

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The attached materials provide the FortisBC Energy Inc. (FEI or the Company) 2022 Second Quarter Gas Cost Report for the Mainland and Vancouver Island Service Area (the Second Quarter Report) as required under the British Columbia Utilities Commission (BCUC) guidelines for gas cost rate setting (the Guidelines)<sup>1</sup>. The gas cost forecast used within the attached report is based on the five-day average of the May 10, 11, 12, 13, and 16, 2022 forward prices (five-day average forward prices ending May 16, 2022).

#### **CCRA Deferral Account and Commodity Rate Setting Mechanism**

Based on the five-day average forward prices ending May 16, 2022, the June 30, 2022 CCRA balance is projected to be approximately \$101 million deficit after tax. At the existing commodity rate, the CCRA trigger ratio is calculated to be 59.3 percent, which falls outside the deadband range of 95 percent to 105 percent. The tested rate increase that would produce a 100 percent commodity recovery-to-cost ratio is calculated to be \$3.088/GJ, which exceeds the \$0.50/GJ minimum rate change threshold. The results of the two-criterion rate adjustment mechanism indicate that a rate change is required.

The indicated rate change, based on the standard 12-month prospective rate setting period, would increase the commodity rate from the existing \$4.503/GJ to \$7.591/GJ effective July 1, 2022, and equates to an annual bill for a typical Mainland and Vancouver Island residential

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<sup>1</sup> The BCUC established guidelines for gas cost rate setting in Letter L-5-01, dated February 5, 2001, and further modified the guidelines pursuant to Letter L-40-11, dated May 19, 2011, and Letter L-15-16, dated June 16, 2016.

customer with an average annual consumption of 90 GJ per year increasing by approximately \$278 or 23.6 percent.

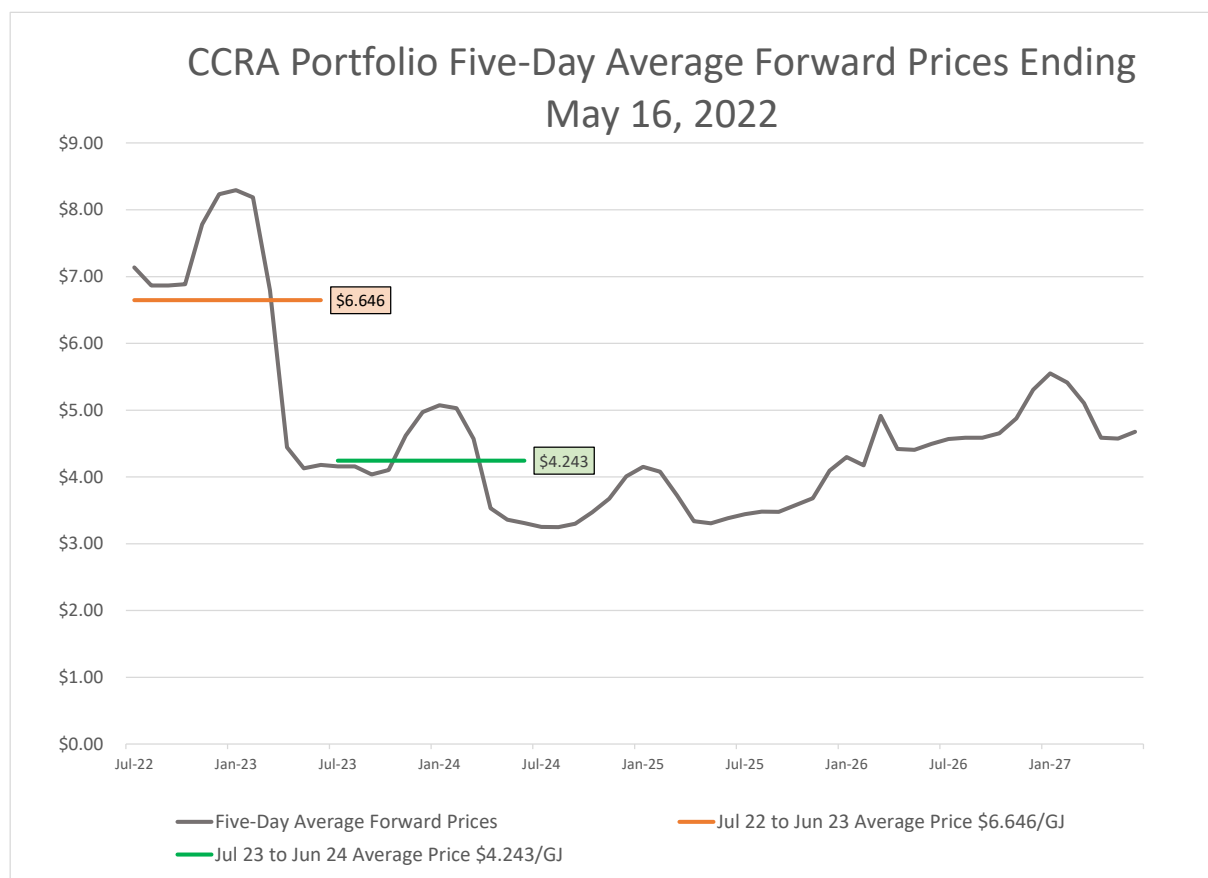
The schedules at Tab 2, Pages 1 and 2, provide details of the recorded and forecast, based on the five-day average forward prices ending May 16, 2022, CCRA gas supply costs. The schedule at Tab 2, Page 3 provides the information related to the unitization of the forecast CCRA gas supply costs for the July 1, 2022 to June 30, 2023 prospective period.

## Discussion

The forward western Canadian natural gas prices have increased significantly from the forward prices used in the FEI 2022 First Quarter Gas Cost Report for the Mainland and Vancouver Island Service Area, when the commodity cost recovery rate did not require a change despite forward prices rising. The forward prices have experienced continued, and large, increases due to stronger prices at Henry Hub, which is the pricing point for natural gas futures across North America. In addition, increasing demand for natural gas and western Canadian storage inventory well below the 5 year average have contributed to the increase in prices.

As shown in Figure 1, the forward prices for the prospective 12 months, July 2022 to June 2023, are about 56.6% higher than those for the following 12 month period, July 2023 to June 2024. The market appears to support a moderating of current market prices after winter 2022/23.

**Figure 1: CCRA Portfolio Forward Prices**



Letter L-15-16 provides criteria for clarification of when consideration is to be given to commodity rate proposals beyond the standard 12-month prospective period. The circumstances reflected in the CCRA portfolio weighted average cost of gas (WACOG), based on the five-day average forward prices ending May 16, 2022, meet all of the criteria identified to assist the BCUC when considering commodity rate change proposals using a 24-month rate setting period rather than the standard 12-month prospective period. The approved criteria, and the results for each, are as follows:

- When a commodity rate change is indicated using a standard 12-month prospective period.
  - The indicated rate change, based on the standard 12-month prospective rate setting period, is a rate increase of \$3.088/GJ effective July 1, 2022.
- When there is a difference of \$0.75/GJ or more between the CCRA WACOG for year one versus year two of the 24-month prospective period.
  - The year one CCRA WACOG is \$6.646/GJ, and the year two CCRA WACOG is \$4.243/GJ. A difference of \$2.403/GJ.
- When the direction of the commodity rate change indicated using a standard 12-month prospective period is opposite to the direction of the CCRA WACOG for year two compared to the CCRA WACOG for year one.
  - Based on using a standard 12-month prospective period and comparing the rate changes indicated for year one (July 2022 to June 2023) compared to year two (July 2023 to June 2024), the year one commodity rate increase is calculated to be \$3.088/GJ, increasing the commodity rate from \$4.503/GJ to \$7.591/GJ effective July 1, 2022. The year two commodity rate would, based on a July 1, 2023 effective date, decrease from \$7.591/GJ to the year two CCRA WACOG amount of \$4.243/GJ, a decrease of \$3.348/GJ.

### **Commodity Rate Setting Alternative Based on 24-Month Outlook**

FEI notes, as discussed above, that the forecast data and circumstances within the Second Quarter Gas Cost Report fully meet the approved criteria of when it may be appropriate to set commodity rate changes utilizing a 24-month prospective period.

Attached at Alternative A – Tabs 3, 5, and 6, are schedules showing the forecast deferral account balances and bill impacts based on the flow through of a \$1.404/GJ increase, calculated using a 24-month prospective rate setting period (as shown at Alternative A – Tab 1, Page 2). The indicated rate change, based on a 24-month prospective rate setting period, would increase the commodity rate from the existing \$4.503/GJ to \$5.907/GJ effective July 1, 2022, and equates to an annual bill for a typical Mainland and Vancouver Island residential customer with an average annual consumption of 90 GJ per year increasing by approximately \$126 or 10.7 percent.

The Company proposes to flow through a \$1.404/GJ commodity rate increase, based on the 24-month prospective rate setting mechanism to address the current CCRA deficit and the prospective under recovery of gas costs in a timely manner, while remaining mindful of the impact to customers' bills.

### **MCRA Deferral Account**

Based on the five-day average forward prices ending May 16, 2022 and existing rates, the MCRA balances after tax at December 31, 2022 and December 31, 2023 are projected to be approximately \$57 million surplus and \$11 million deficit, respectively. The monthly MCRA deferral account balances are shown on the schedule provided at Tab 1, Page 3.

The schedules at Tab 2, Pages 4 and 5, provide details of MCRA gas supply costs for calendar 2022, and 2023 based on the five-day average forward prices ending May 16, 2022.

The schedules at Tab 4, Pages 1 to 4 provide details of the forecast costs for Revelstoke propane supply. FEI requests the information contained within Tab 4 be treated as CONFIDENTIAL.

FEI will continue to monitor and report the MCRA balances consistent with the Company's position that midstream costs and recoveries be reported on a quarterly basis and, under normal circumstances, the storage and transport rates be adjusted on an annual basis with a January 1 effective date.

### **CONFIDENTIALITY**

FEI is requesting that this information be filed on a confidential basis pursuant to Section 18 of the BCUC's Rules of Practice and Procedure regarding confidential documents adopted by Order G-15-19, and Section 71(5) of the *Utilities Commission Act* and requests that the BCUC exercise its discretion under Section 6.0 of the Rules for Natural Gas Energy Supply Contracts and allow these documents to remain confidential. FEI believes this will ensure that market sensitive information is protected, and FEI's ability to obtain favorable commercial terms for future natural gas contracting is not impaired.

In this regard, FEI further believes that the Core Market could be disadvantaged and may well shoulder incremental costs if utility gas supply procurement strategies as well as contracts are treated in a different manner than those of other gas purchasers, and believes that since it continues to operate within a competitive environment, there is no necessity for public disclosure and risk prejudice or influence in the negotiations or renegotiation of subsequent contracts.

### **Summary**

The Company requests approval to flow through a \$1.404/GJ increase to the Commodity Cost Recovery Charge at July 1, 2022, applicable to all affected sales rate classes, including Rate Schedule 46 LNG Service, within the Mainland and Vancouver Island service area.

FEI will continue to monitor the forward prices, and will report CCRA and MCRA balances in its 2022 Third Quarter Gas Cost Report.

We trust BCUC will find the attached to be in order. However, should any further information be required, please contact Gurvinder Sidhu at 604-592-7675.

Sincerely,

**FORTISBC ENERGY INC.**

***Original signed:***

Diane Roy

Attachments

**FORTISBC ENERGY INC. - MAINLAND AND VANCOUVER ISLAND SERVICE AREA**  
**CCRA MONTHLY BALANCES AT EXISTING RATES (AFTER ADJUSTMENTS FOR ENERGY DIFFERENCES)**  
**FOR THE FORECAST PERIOD FROM JUL 2022 TO JUN 2024**  
**FIVE-DAY AVERAGE FORWARD PRICES - MAY 10, 11, 12, 13, AND 16, 2022**  
**\$(Millions)**

Tab 1

Page 1

Line	(1)	(2)	(3)	(4)	(5)	(6)	(7)	(8)	(9)	(10)	(11)	(12)	(13)	(14)
		Recorded Jan-22	Recorded Feb-22	Recorded Mar-22	Recorded Apr-22	Projected May-22	Projected Jun-22							Jan-22 to Jun-22
1														
2	CCRA Balance - Beginning (Pre-tax) <sup>(a)</sup>	\$ 68	\$ 68	\$ 71	\$ 76	\$ 89	\$ 107							<b>\$ 68</b>
3	Gas Costs Incurred	55	54	61	67	75	85							396
4	Revenue from <b>APPROVED</b> Recovery Rate	(56)	(51)	(56)	(54)	(56)	(54)							(327)
5	<b>CCRA Balance - Ending (Pre-tax) <sup>(b)</sup></b>	<b>\$ 68</b>	<b>\$ 71</b>	<b>\$ 76</b>	<b>\$ 89</b>	<b>\$ 107</b>	<b>\$ 138</b>							<b>\$ 138</b>
6														
7	Tax Rate	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%							27.0%
8														
9	<b>CCRA Balance - Ending (After-tax) <sup>(c)</sup></b>	<b>\$ 50</b>	<b>\$ 52</b>	<b>\$ 56</b>	<b>\$ 65</b>	<b>\$ 78</b>	<b>\$ 101</b>							<b>\$ 101</b>
10														
11														Jul-22 to Jun-23
12		Forecast Jul-22	Forecast Aug-22	Forecast Sep-22	Forecast Oct-22	Forecast Nov-22	Forecast Dec-22	Forecast Jan-23	Forecast Feb-23	Forecast Mar-23	Forecast Apr-23	Forecast May-23	Forecast Jun-23	
13														
14	CCRA Balance - Beginning (Pre-tax) <sup>(a)</sup>	\$ 138	\$ 171	\$ 200	\$ 229	\$ 258	\$ 298	\$ 344	\$ 391	\$ 433	\$ 461	\$ 460	\$ 456	<b>\$ 138</b>
15	Gas Costs Incurred	89	85	83	86	94	102	103	92	84	53	51	50	972
16	Revenue from <b>EXISTING</b> Recovery Rates	(56)	(56)	(54)	(56)	(54)	(56)	(56)	(51)	(56)	(54)	(56)	(54)	(659)
17	<b>CCRA Balance - Ending (Pre-tax) <sup>(b)</sup></b>	<b>\$ 171</b>	<b>\$ 200</b>	<b>\$ 229</b>	<b>\$ 258</b>	<b>\$ 298</b>	<b>\$ 344</b>	<b>\$ 391</b>	<b>\$ 433</b>	<b>\$ 461</b>	<b>\$ 460</b>	<b>\$ 456</b>	<b>\$ 452</b>	<b>\$ 452</b>
18														
19	Tax Rate	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%
20														
21	<b>CCRA Balance - Ending (After-tax) <sup>(c)</sup></b>	<b>\$ 125</b>	<b>\$ 146</b>	<b>\$ 167</b>	<b>\$ 189</b>	<b>\$ 217</b>	<b>\$ 251</b>	<b>\$ 286</b>	<b>\$ 316</b>	<b>\$ 337</b>	<b>\$ 336</b>	<b>\$ 333</b>	<b>\$ 330</b>	<b>\$ 330</b>
22														
23														
24														Jul-23 to Jun-24
25		Forecast Jul-23	Forecast Aug-23	Forecast Sep-23	Forecast Oct-23	Forecast Nov-23	Forecast Dec-23	Forecast Jan-24	Forecast Feb-24	Forecast Mar-24	Forecast Apr-24	Forecast May-24	Forecast Jun-24	
26														
27	CCRA Balance - Beginning (Pre-tax) <sup>(a)</sup>	\$ 452	\$ 447	\$ 443	\$ 438	\$ 433	\$ 434	\$ 440	\$ 447	\$ 453	\$ 454	\$ 442	\$ 428	<b>\$ 452</b>
28	Gas Costs Incurred	52	52	49	51	56	62	64	59	57	43	42	40	628
29	Revenue from <b>EXISTING</b> Recovery Rates	(56)	(56)	(55)	(56)	(55)	(56)	(56)	(53)	(56)	(55)	(56)	(55)	(667)
30	<b>CCRA Balance - Ending (Pre-tax) <sup>(b)</sup></b>	<b>\$ 447</b>	<b>\$ 443</b>	<b>\$ 438</b>	<b>\$ 433</b>	<b>\$ 434</b>	<b>\$ 440</b>	<b>\$ 447</b>	<b>\$ 453</b>	<b>\$ 454</b>	<b>\$ 442</b>	<b>\$ 428</b>	<b>\$ 413</b>	<b>\$ 413</b>
31														
32	Tax Rate	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%
33														
34	<b>CCRA Balance - Ending (After-tax) <sup>(c)</sup></b>	<b>\$ 327</b>	<b>\$ 324</b>	<b>\$ 319</b>	<b>\$ 316</b>	<b>\$ 317</b>	<b>\$ 321</b>	<b>\$ 326</b>	<b>\$ 331</b>	<b>\$ 331</b>	<b>\$ 323</b>	<b>\$ 312</b>	<b>\$ 302</b>	<b>\$ 302</b>

Notes:

(a) Pre-tax opening balances are restated based on current income tax rates, to reflect grossed-up after tax amounts.

(b) For rate setting purposes CCRA pre-tax balances include grossed-up projected deferred interest of approximately \$0.5 million as at June 30, 2022.

(c) For rate setting purposes CCRA after-tax balances are independently grossed-up to reflect pre-tax amounts.

Slight differences in totals due to rounding.

**FORTISBC ENERGY INC. - MAINLAND AND VANCOUVER ISLAND SERVICE AREA**  
**CCRA RATE CHANGE TRIGGER MECHANISM**  
**FOR THE FORECAST PERIOD JUL 2022 TO JUN 2023**  
**FIVE-DAY AVERAGE FORWARD PRICES - MAY 10, 11, 12, 13, AND 16, 2022**

Tab 1

Page 2

Line	Particulars	Pre-Tax (\$Millions)	Forecast Energy (TJ)	Percentage	Unit Cost (\$/GJ)	Reference / Comment
	(1)	(2)	(3)	(4)	(5)	(6)
1	<b><u>CCRA RATE CHANGE TRIGGER RATIO</u></b>					
2	(a)					
3	Projected Deferral Balance at Jul 1, 2022	\$ 138.3				(Tab 1, Page 1, Col.14, Line 14)
4	Forecast Incurred Gas Costs - Jul 2022 to Jun 2023	\$ 972.3				(Tab 1, Page 1, Col.14, Line 15)
5	Forecast Recovery Gas Costs at Existing Recovery Rate - Jul 2022 to Jun 2023	\$ 658.8				(Tab 1, Page 1, Col.14, Line 16)
6						
7	<b>CCRA =</b> Forecast Recovered Gas Costs (Line 5)	= \$ 658.8		= <b>59.3%</b>		
8	<b>Ratio</b> Forecast Incurred Gas Costs (Line 4) + Projected CCRA Balance (Line 3)	\$ 1,110.6				<b>Outside 95% to 105% deadband</b>
9						
10						
11						
12						
13	<b><u>Existing Cost of Gas (Commodity Cost Recovery Rate), effective January 1, 2022</u></b>				<b>\$ 4.503</b>	
14						
15						
16						
17						
18	<b><u>CCRA RATE CHANGE THRESHOLD (+/- \$0.50/GJ)</u></b>					
19						
20	Forecast 12-month CCRA Baseload - Jul 2022 to Jun 2023		146,293			(Tab1, Page 7, Col.5, Line 11 )
21						
22	Projected Deferral Balance at Jul 1, 2022 (a)	\$ 138.3			\$ 0.9451 (b)	
23	Forecast 12-month CCRA Activities - Jul 2022 to Jun 2023	\$ 313.5			\$ 2.1433 (b)	
24	(Over) / Under Recovery at Existing Rate	\$ 451.8				(Line 3 + Line 4 - Line 5)
25						
26	<b>Tested Rate (Decrease) / Increase</b>				<b>\$ 3.088 (b)</b>	<b>Exceeds minimum +/- \$0.50/GJ threshold &amp; +/- \$1.00/GJ cap</b>

Notes:

(a) Pre-tax opening balances are restated based on current income tax rates, to reflect grossed-up after tax amounts.

(b) Commodity cost recovery rate in tariff is set at 3 decimal places. Individual rate components are shown to 4 decimals places.

Slight differences in totals due to rounding.

**FORTISBC ENERGY INC. - MAINLAND AND VANCOUVER ISLAND SERVICE AREA**  
**MCRA MONTHLY BALANCES AT EXISTING RATES (AFTER ADJUSTMENTS FOR ENERGY DIFFERENCES)**  
**FOR THE FORECAST PERIOD FROM JUL 2022 TO DEC 2023**  
**FIVE-DAY AVERAGE FORWARD PRICES - MAY 10, 11, 12, 13, AND 16, 2022**  
**\$(Millions)**

Tab 1  
Page 3

Line	(1)	(2)	(3)	(4)	(5)	(6)	(7)	(8)	(9)	(10)	(11)	(12)	(13)	(14)
		Recorded Jan-22	Recorded Feb-22	Recorded Mar-22	Recorded Apr-22	Projected May-22	Projected Jun-22	Forecast Jul-22	Forecast Aug-22	Forecast Sep-22	Forecast Oct-22	Forecast Nov-22	Forecast Dec-22	Total 2022
1	MCRA Balance - Beginning (Pre-tax) <sup>(a)</sup>	\$ (40)	\$ (58)	\$ (66)	\$ (72)	\$ (80)	\$ (78)	\$ (85)	\$ (94)	\$ (101)	\$ (106)	\$ (106)	\$ (97)	\$ (40)
2														
3	<b>2022 MCRA Activities</b>													
4	Rate Rider 6													
5	Approved Amount to be amortized in 2022													\$ (22)
6	Rider 6 Amortization at APPROVED 2022 Rates	\$ 3	\$ 3	\$ 2	\$ 2	\$ 1	\$ 1	\$ 1	\$ 1	\$ 1	\$ 2	\$ 3	\$ 4	\$ 22
7	Midstream Base Rates													
8	Gas Costs Incurred	\$ 66	\$ 54	\$ 33	\$ 18	\$ (13)	\$ (28)	\$ (39)	\$ (39)	\$ (27)	\$ 6	\$ 52	\$ 100	\$ 182
9	Revenue from <b>APPROVED</b> Recovery Rates	(86)	(64)	(42)	(28)	14	22	30	31	21	(8)	(45)	(84)	(239)
10	Total Midstream Base Rates (Pre-tax)	\$ (21)	\$ (10)	\$ (9)	\$ (9)	\$ 0	\$ (6)	\$ (9)	\$ (8)	\$ (6)	\$ (2)	\$ 7	\$ 15	\$ (57)
11														
12														
13	<b>MCRA Cumulative Balance - Ending (Pre-tax) <sup>(b)</sup></b>	\$ (58)	\$ (66)	\$ (72)	\$ (80)	\$ (78)	\$ (85)	\$ (94)	\$ (101)	\$ (106)	\$ (106)	\$ (97)	\$ (78)	\$ (78)
14	Tax Rate	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%
15														
16	<b>MCRA Cumulative Balance - Ending (After-tax) <sup>(c)</sup></b>	\$ (42)	\$ (48)	\$ (53)	\$ (58)	\$ (57)	\$ (62)	\$ (69)	\$ (74)	\$ (77)	\$ (77)	\$ (71)	\$ (57)	\$ (57)
17														
18														
19		Forecast Jan-23	Forecast Feb-23	Forecast Mar-23	Forecast Apr-23	Forecast May-23	Forecast Jun-23	Forecast Jul-23	Forecast Aug-23	Forecast Sep-23	Forecast Oct-23	Forecast Nov-23	Forecast Dec-23	Total 2023
20														
21	MCRA Balance - Beginning (Pre-tax) <sup>(a)</sup>	\$ (78)	\$ (60)	\$ (47)	\$ (40)	\$ (35)	\$ (24)	\$ (12)	\$ 2	\$ 15	\$ 27	\$ 29	\$ 26	\$ (78)
22	<b>2023 MCRA Activities</b>													
23	Rate Rider 6													
24														
25	Rider 6 Amortization at APPROVED 2022 Rates	\$ 3	\$ 3	\$ 2	\$ 2	\$ 1	\$ 1	\$ 1	\$ 1	\$ 1	\$ 2	\$ 3	\$ 4	\$ 23
26	Midstream Base Rates													
27	Gas Costs Incurred	\$ 96	\$ 75	\$ 45	\$ 18	\$ (4)	\$ (10)	\$ (17)	\$ (19)	\$ (10)	\$ 9	\$ 40	\$ 70	\$ 293
28	Revenue from <b>EXISTING</b> Recovery Rates	(82)	(64)	(40)	(15)	13	22	30	31	21	(8)	(46)	(85)	(223)
29	Total Midstream Base Rates (Pre-tax)	\$ 14	\$ 11	\$ 5	\$ 3	\$ 9	\$ 12	\$ 13	\$ 12	\$ 11	\$ 1	\$ (6)	\$ (15)	\$ 70
30														
31	<b>MCRA Cumulative Balance - Ending (Pre-tax) <sup>(b)</sup></b>	\$ (60)	\$ (47)	\$ (40)	\$ (35)	\$ (24)	\$ (12)	\$ 2	\$ 15	\$ 27	\$ 29	\$ 26	\$ 14	\$ 14
32	Tax Rate	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%
33														
34	<b>MCRA Cumulative Balance - Ending (After-tax) <sup>(c)</sup></b>	\$ (44)	\$ (34)	\$ (29)	\$ (26)	\$ (18)	\$ (9)	\$ 1	\$ 11	\$ 20	\$ 21	\$ 19	\$ 11	\$ 11

Notes:

- (a) Pre-tax opening balances are restated based on current income tax rates, to reflect grossed-up after tax amounts.  
(b) For rate setting purposes MCRA pre-tax balances include grossed-up projected deferred interest of approximately \$2.3 million credit as at June 30, 2022.  
(c) For rate setting purposes MCRA after-tax balances are independently grossed-up to reflect pre-tax amounts.

Slight differences in totals due to rounding.



**FORTISBC ENERGY INC. - MAINLAND AND VANCOUVER ISLAND SERVICE AREA**  
**SUMAS INDEX FORECAST FOR THE PERIOD ENDING JUN 2024**  
**AND US DOLLAR EXCHANGE RATE FORECAST UPDATE**

Tab 1  
Page 4.1

Line No	Particulars	Five-day Average Forward Prices - May 10, 11, 12, 13, and 16, 2022 2022 Q2 Gas Cost Report	Five-day Average Forward Prices - February 14, 15, 16, 17, and 18, 2022 2022 Q1 Gas Cost Report	Change in Forward Price
	(1)	(2)	(3)	(4) = (2) - (3)
1	<b>SUMAS Index Prices - presented in \$US/MMBtu</b>			
2				
3	<b>2022</b> January	↑ \$ 10.81	\$ 10.81	\$ -
4	February	\$ 5.00	<b>Settled</b> \$ 5.00	\$ -
5	March	\$ 4.30	<b>Forecast</b> \$ 3.99	\$ 0.31
6	April	<b>Settled</b> \$ 4.76	↓ \$ 3.91	\$ 0.85
7	May	\$ 6.18	\$ 3.65	\$ 2.52
8	June	\$ 7.12	\$ 3.74	\$ 3.38
9	July	\$ 7.51	\$ 4.07	\$ 3.45
10	August	\$ 7.99	\$ 4.78	\$ 3.22
11	September	\$ 7.67	\$ 4.29	\$ 3.38
12	October	\$ 7.60	\$ 4.36	\$ 3.24
13	November	\$ 8.44	\$ 5.26	\$ 3.18
14	December	\$ 9.55	\$ 6.05	\$ 3.50
15	<b>2023</b> January	\$ 9.72	\$ 6.13	\$ 3.59
16	February	\$ 9.18	\$ 5.78	\$ 3.39
17	March	\$ 6.33	\$ 4.28	\$ 2.06
18	April	\$ 4.07	\$ 2.75	\$ 1.32
19	May	\$ 3.70	\$ 2.54	\$ 1.16
20	June	\$ 3.80	\$ 2.63	\$ 1.17
21	July	\$ 4.28	\$ 3.07	\$ 1.21
22	August	\$ 4.36	\$ 3.13	\$ 1.23
23	September	\$ 4.27	\$ 3.14	\$ 1.14
24	October	\$ 4.25	\$ 3.13	\$ 1.12
25	November	\$ 5.41	\$ 4.20	\$ 1.21
26	December	\$ 6.50	\$ 5.07	\$ 1.43
27	<b>2024</b> January	\$ 6.42	\$ 4.83	\$ 1.59
28	February	\$ 5.93	\$ 4.49	\$ 1.44
29	March	\$ 4.71	\$ 3.69	\$ 1.01
30	April	\$ 3.26		
31	May	\$ 3.16		
32	June	\$ 3.23		
33				
34	Simple Average (Jul 2022 - Jun 2023)	\$ 7.13	\$ 4.41	61.7% \$ 2.72
35	Simple Average (Oct 2022 - Sep 2023)	\$ 6.28	\$ 4.09	53.3% \$ 2.18
36	Simple Average (Jan 2023 - Dec 2023)	\$ 5.49	\$ 3.82	43.7% \$ 1.67
37	Simple Average (Apr 2023 - Mar 2024)	\$ 4.81	\$ 3.56	35.2% \$ 1.25
38	Simple Average (Jul 2023 - Jun 2024)	\$ 4.65		

Conversion Factors

1 MMBtu = 1.055056 GJ

Morningstar Average Exchange Rate (\$1US=\$x.xxxCDN)

Forecast Jul 2022 - Jun 2023  
\$ 1.2947

Forecast Apr 2022 - Mar 2023  
\$ 1.2723

1.8% \$ 0.0224

**FORTISBC ENERGY INC. - MAINLAND AND VANCOUVER ISLAND SERVICE AREA**  
**SUMAS INDEX FORECAST FOR THE PERIOD ENDING JUN 2024**

Tab 1  
Page 4.2

Line No	Particulars	Five-day Average Forward Prices - May 10, 11, 12, 13, and 16, 2022	Five-day Average Forward Prices - February 14, 15, 16, 17, and 18, 2022	Change in Forward Price
	(1)	2022 Q2 Gas Cost Report (2)	2022 Q1 Gas Cost Report (3)	(4) = (2) - (3)
1	<b>SUMAS Index Prices - presented in \$CDN/GJ</b>			
2				
3	<b>2022</b>			
4	January	\$ 13.09	\$ 13.09	\$ -
5	February	\$ 6.03	\$ 6.03	\$ -
6	March	\$ 5.18	\$ 4.81	\$ 0.37
7	April	\$ 5.64	\$ 4.72	\$ 0.92
8	May	\$ 7.52	\$ 4.40	\$ 3.11
9	June	\$ 8.74	\$ 4.51	\$ 4.23
10	July	\$ 9.22	\$ 4.90	\$ 4.32
11	August	\$ 9.81	\$ 5.76	\$ 4.05
12	September	\$ 9.42	\$ 5.18	\$ 4.24
13	October	\$ 9.33	\$ 5.25	\$ 4.08
14	November	\$ 10.36	\$ 6.35	\$ 4.01
15	December	\$ 11.73	\$ 7.30	\$ 4.43
16	<b>2023</b>			
17	January	\$ 11.94	\$ 7.40	\$ 4.54
18	February	\$ 11.27	\$ 6.97	\$ 4.30
19	March	\$ 7.78	\$ 5.16	\$ 2.62
20	April	\$ 5.00	\$ 3.32	\$ 1.69
21	May	\$ 4.51	\$ 3.06	\$ 1.45
22	June	\$ 4.66	\$ 3.17	\$ 1.50
23	July	\$ 5.26	\$ 3.71	\$ 1.55
24	August	\$ 5.35	\$ 3.77	\$ 1.58
25	September	\$ 5.25	\$ 3.78	\$ 1.47
26	October	\$ 5.22	\$ 3.78	\$ 1.45
27	November	\$ 6.65	\$ 5.07	\$ 1.58
28	December	\$ 7.98	\$ 6.12	\$ 1.87
29	<b>2024</b>			
30	January	\$ 7.89	\$ 5.82	\$ 2.07
31	February	\$ 7.28	\$ 5.41	\$ 1.87
32	March	\$ 5.78	\$ 4.45	\$ 1.33
33	April	\$ 4.00		
34	May	\$ 3.88		
35	June	\$ 3.93		
36	Simple Average (Jul 2022 - Jun 2023)	\$ 8.75	\$ 5.32	64.6% \$ 3.44
37	Simple Average (Oct 2022 - Sep 2023)	\$ 7.70	\$ 4.94	56.1% \$ 2.77
38	Simple Average (Jan 2023 - Dec 2023)	\$ 6.74	\$ 4.61	46.3% \$ 2.13
39	Simple Average (Apr 2023 - Mar 2024)	\$ 5.90	\$ 4.29	37.7% \$ 1.62
40	Simple Average (Jul 2023 - Jun 2024)	\$ 5.71		

Conversion Factors

1 MMBtu = 1.055056 GJ

Morningstar Average Exchange Rate (\$1US=\$x.xxxCDN)

Forecast Jul 2022 - Jun 2023  
\$ 1.2947

Forecast Apr 2022 - Mar 2023  
\$ 1.2723

1.8% \$ 0.0224

**FORTISBC ENERGY INC. - MAINLAND AND VANCOUVER ISLAND SERVICE AREA**  
**AECO INDEX FORECAST FOR THE PERIOD ENDING JUN 2024**

Tab 1  
Page 5

		Five-day Average Forward Prices - May 10, 11, 12, 13, and 16, 2022		Five-day Average Forward Prices - February 14, 15, 16, 17, and 18, 2022		Change in Forward Price	
Line No	Particulars	2022 Q2 Gas Cost Report		2022 Q1 Gas Cost Report		(4) = (2) - (3)	
	(1)	(2)		(3)			
1	AECO Index Prices - \$CDN/GJ						
2							
3	2022	January	↑ \$ 4.10	Settled \$ 4.10		\$ -	
4		February	\$ 4.46	Forecast \$ 4.46		\$ -	
5		March	\$ 4.49	\$ 4.33		\$ 0.16	
6		April	\$ 4.56	\$ 4.08		\$ 0.48	
7		May	\$ 5.86	\$ 4.00		\$ 1.86	
8		June	\$ 6.83	\$ 3.96		\$ 2.87	
9		July	\$ 6.85	\$ 3.98		\$ 2.87	
10		August	\$ 6.62	\$ 3.83		\$ 2.79	
11		September	\$ 6.65	\$ 3.91		\$ 2.74	
12		October	\$ 6.67	\$ 4.01		\$ 2.67	
13		November	\$ 7.35	\$ 4.26		\$ 3.10	
14		December	\$ 7.73	\$ 4.42		\$ 3.32	
15	2023	January	\$ 7.76	\$ 4.45		\$ 3.31	
16		February	\$ 7.69	\$ 4.39		\$ 3.30	
17		March	\$ 6.49	\$ 3.94		\$ 2.54	
18		April	\$ 4.38	\$ 3.06		\$ 1.32	
19		May	\$ 4.09	\$ 2.84		\$ 1.25	
20		June	\$ 4.13	\$ 2.84		\$ 1.30	
21		July	\$ 4.07	\$ 2.86		\$ 1.21	
22		August	\$ 4.07	\$ 2.88		\$ 1.19	
23		September	\$ 3.95	\$ 2.97		\$ 0.98	
24		October	\$ 4.02	\$ 3.10		\$ 0.92	
25		November	\$ 4.42	\$ 3.34		\$ 1.07	
26		December	\$ 4.76	\$ 3.53		\$ 1.22	
27	2024	January	\$ 4.86	\$ 3.63		\$ 1.22	
28		February	\$ 4.81	\$ 3.58		\$ 1.23	
29		March	\$ 4.37	\$ 3.30		\$ 1.08	
30		April	\$ 3.48				
31		May	\$ 3.32				
32		June	\$ 3.27				
33							
34	Simple Average (Jul 2022 - Jun 2023)		\$ 6.37	\$ 3.83	66.4%	\$ 2.54	
35	Simple Average (Oct 2022 - Sep 2023)		\$ 5.70	\$ 3.58	59.4%	\$ 2.12	
36	Simple Average (Jan 2023 - Dec 2023)		\$ 4.99	\$ 3.35	48.8%	\$ 1.63	
37	Simple Average (Apr 2023 - Mar 2024)		\$ 4.33	\$ 3.16	36.9%	\$ 1.17	
38	Simple Average (Jul 2023 - Jun 2024)		\$ 4.12				

**FORTISBC ENERGY INC. - MAINLAND AND VANCOUVER ISLAND SERVICE AREA**  
**STATION 2 INDEX FORECAST FOR THE PERIOD ENDING JUN 2024**

Tab 1  
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Line No	Particulars	Five-day Average Forward Prices - May 10, 11, 12, 13, and 16, 2022 2022 Q2 Gas Cost Report	Five-day Average Forward Prices - February 14, 15, 16, 17, and 18, 2022 2022 Q1 Gas Cost Report	Change in Forward Price (4) = (2) - (3)
	(1)	(2)	(3)	
1	<b>Station 2 Index Prices - \$CDN/GJ</b>			
2				
3	<b>2022</b> January	↑ \$ 4.51	Settled \$ 4.51	\$ -
4	February	\$ 4.49	Forecast \$ 4.49	\$ -
5	March	\$ 4.52	\$ 4.18	\$ 0.34
6	April	\$ 4.74	\$ 3.88	\$ 0.85
7	May	\$ 5.41	\$ 3.80	\$ 1.60
8	June	\$ 6.43	\$ 3.76	\$ 2.67
9	July	\$ 6.60	\$ 3.78	\$ 2.81
10	August	\$ 6.11	\$ 3.43	\$ 2.68
11	September	\$ 6.14	\$ 3.51	\$ 2.63
12	October	\$ 6.16	\$ 3.61	\$ 2.55
13	November	\$ 7.38	\$ 4.25	\$ 3.13
14	December	\$ 7.76	\$ 4.41	\$ 3.35
15	<b>2023</b> January	\$ 7.82	\$ 4.44	\$ 3.38
16	February	\$ 7.75	\$ 4.38	\$ 3.37
17	March	\$ 6.55	\$ 3.93	\$ 2.61
18	April	\$ 4.20	\$ 2.97	\$ 1.23
19	May	\$ 3.91	\$ 2.75	\$ 1.16
20	June	\$ 3.95	\$ 2.74	\$ 1.21
21	July	\$ 3.89	\$ 2.77	\$ 1.13
22	August	\$ 3.89	\$ 2.78	\$ 1.10
23	September	\$ 3.77	\$ 2.88	\$ 0.89
24	October	\$ 3.84	\$ 3.01	\$ 0.83
25	November	\$ 4.44	\$ 3.33	\$ 1.10
26	December	\$ 4.78	\$ 3.52	\$ 1.25
27	<b>2024</b> January	\$ 4.88	\$ 3.62	\$ 1.25
28	February	\$ 4.83	\$ 3.57	\$ 1.26
29	March	\$ 4.39	\$ 3.29	\$ 1.11
30	April	\$ 3.34		
31	May	\$ 3.18		
32	June	\$ 3.13		
33				
34	Simple Average (Jul 2022 - Jun 2023)	\$ 6.19	\$ 3.68	68.1% \$ 2.51
35	Simple Average (Oct 2022 - Sep 2023)	\$ 5.59	\$ 3.49	59.9% \$ 2.09
36	Simple Average (Jan 2023 - Dec 2023)	\$ 4.90	\$ 3.29	48.8% \$ 1.61
37	Simple Average (Apr 2023 - Mar 2024)	\$ 4.23	\$ 3.10	36.3% \$ 1.13
38	Simple Average (Jul 2023 - Jun 2024)	\$ 4.03		

**FORTISBC ENERGY INC. - MAINLAND AND VANCOUVER ISLAND SERVICE AREA**  
**GAS BUDGET COST SUMMUARY FOR THE FORECAST PERIOD JUL 2022 TO JUN 2023**  
**FIVE-DAY AVERAGE FORWARD PRICES - MAY 10, 11, 12, 13, AND 16, 2022**

Tab 1  
Page 7

Line	Particulars	Costs (\$000)		Quantities (TJ)		Unit Cost (\$/GJ)	Reference / Comments	
	(1)	(2)	(3)	(4)	(5)	(6)	(7)	(8)
1	CCRA							
2	Commodity							
3	STN 2		\$ 734,701		114,986		\$ 6.389	
4	AECO		235,928		37,049		\$ 6.368	
5	Commodity Costs before Hedging		\$ 970,629		152,035		\$ 6.384	Incl. Receipt Point Fuel.
6	Hedging Cost / (Gain)		-		-			
7	Subtotal Commodity Purchased		\$ 970,629		152,035		\$ 6.384	
8	Core Market Administration Costs		1,673		-			
9	Fuel Gas Provided to Midstream				(5,742)			
10	Total CCRA Baseload				146,293			
11	Total CCRA Costs		\$ 972,302				\$ 6.646	Commodity available for sale average unit cost
12	MCRA							
13	Midstream Commodity Related Costs							
14	Total Cost of Propane	\$ 5,331				297		
15	Propane Costs Recovered based on Commodity Rates	(2,153)				(284)		
16	Propane Costs to be Recovered via Midstream Rates		\$ 3,178					
17	Midstream Natural Gas Costs before Hedging		171,414		23,643			
18	Hedging Cost / (Gain)		-		-			
19	Imbalance		(2,237)		(412)			
20	Company Use Gas Recovered from O&M		(5,344)		(701)			
21	Injections into Storage	\$ (192,818)		(34,244)				
22	Withdrawals from Storage	200,574		32,215				
23	Storage Withdrawal / (Injection) Activity		7,756		(2,029)			
24	Total Midstream Commodity Related Costs		\$ 174,768		20,501			
25								
26	Storage Related Costs							
27	Storage Demand - Third Party Storage	\$ 40,032						
28	On-System Storage - Mt. Hayes (LNG)	18,928						
29	Total Storage Related Costs		58,960					
30								
31	Transport Related Costs		211,562					
32								
33	Mitigation							
34	Commodity Mitigation	\$ (141,903)			(23,262)			
35	Storage Mitigation	-						
36	Transportation Mitigation	(36,494)						
37	Total Mitigation		(178,396)					
38								
39	GSMIP Incentive Sharing		1,000					
40								
41	Core Market Administration Costs		3,902					
42								
43	Net Transportation Fuel <sup>(a)</sup>			4,040				
44	UAF (Sales and T-Service) <sup>(b)</sup>			(1,279)				
45	UAF & Net Transportation Fuel				2,761			
46	Propane Own Use and UAF					(13)		
47	Net MCRA Commodity (Lines 24, 34 & 45)				-			
48	Total MCRA Costs (Lines 24, 29, 31, 37, 39, & 41)		\$ 271,796				\$ 1.732	Midstream average unit cost
49	Total Sales Quantities for RS1-RS7 & RS46 (Natural Gas & Propane)				156,892			Reference to Tab 2, Page 6, Line 1, Col. 7
50	Total Forecast Gas Costs (Lines 12 & 48)		\$ 1,244,097					Reference to Tab 1, Page 8, Line 11, Col. 3

Notes: (a) Net Transportation Fuel is the difference between fuel gas collected from Commodity Providers and the fuel gas consumed.

(b) The total cost of UAF (Sales Rate Classes and T-Service) is included as a component of gas purchased. Sales UAF costs are recovered via gas cost recovery rates.

As the T-Service UAF costs are recovered via delivery revenues, they are excluded from the storage and transport rate flow-through calculation.

Slight differences in totals due to rounding.

**FORTISBC ENERGY INC. - MAINLAND AND VANCOUVER ISLAND SERVICE AREA**  
**RECONCILIATION OF GAS COST INCURRED**  
**FOR THE FORECAST PERIOD JUL 2022 TO JUN 2023**  
**FIVE-DAY AVERAGE FORWARD PRICES - MAY 10, 11, 12, 13, AND 16, 2022**  
**\$(Millions)**

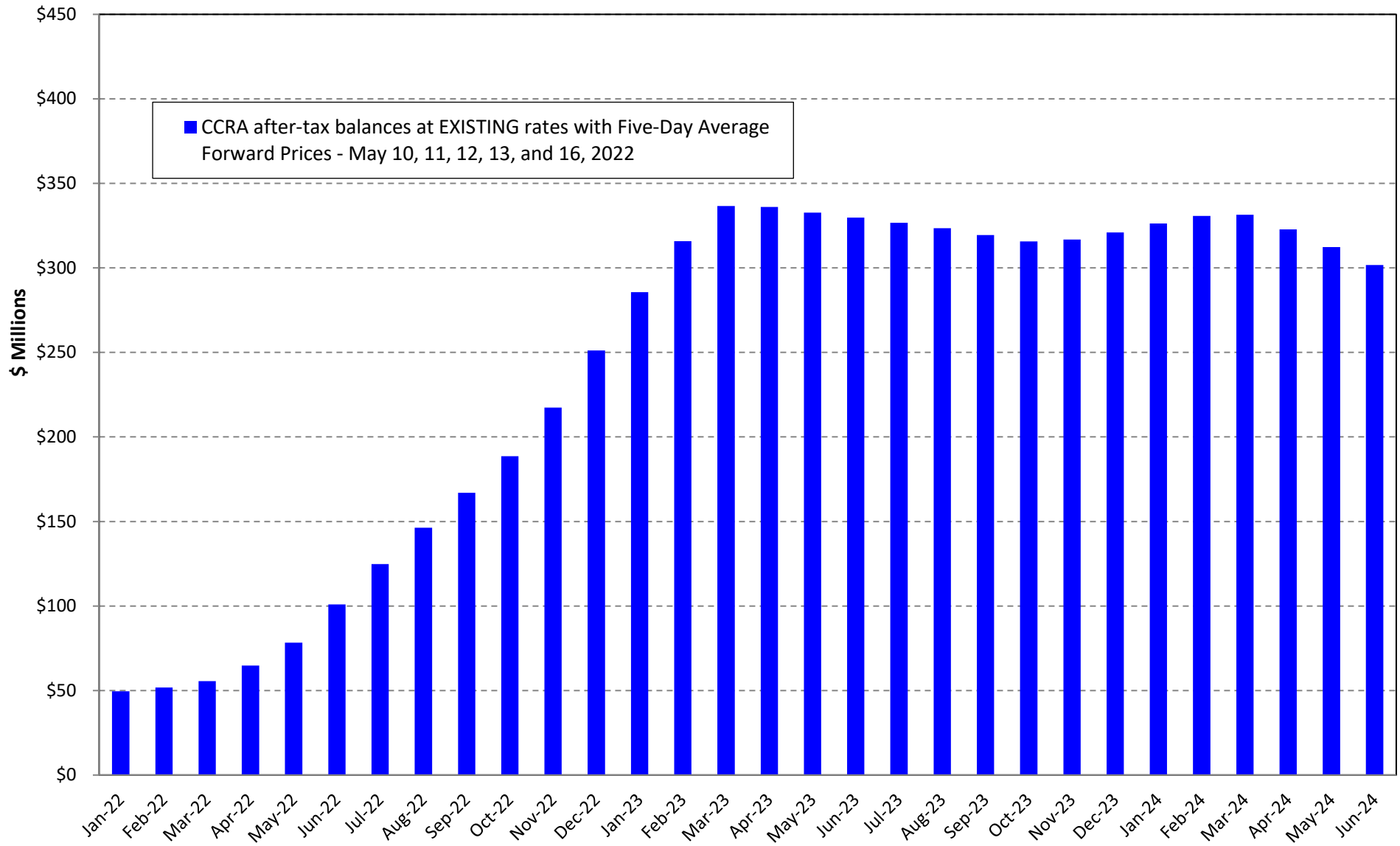
Tab 1  
Page 8

Line	Particulars	CCRA / MCRA Deferral Account Forecast	Gas Budget Cost Summary	References
	(1)	(2)	(3)	(4)
1	Gas Cost Incurred			
2	CCRA	\$ 972		(Tab 1, Page 1, Col.14, Line 15 )
3	MCRA	272		(Tab 2, Page 6.1, Col.15, Line 37)
4				
5				
6	Gas Budget Cost Summary			
7	CCRA		\$ 972	(Tab 1, Page 7, Col.3, Line 11)
8	MCRA		272	(Tab 1, Page 7, Col.3, Line 48)
9				
10				
11	<b>Totals Reconciled</b>	<b>\$ 1,244</b>	<b>\$ 1,244</b>	

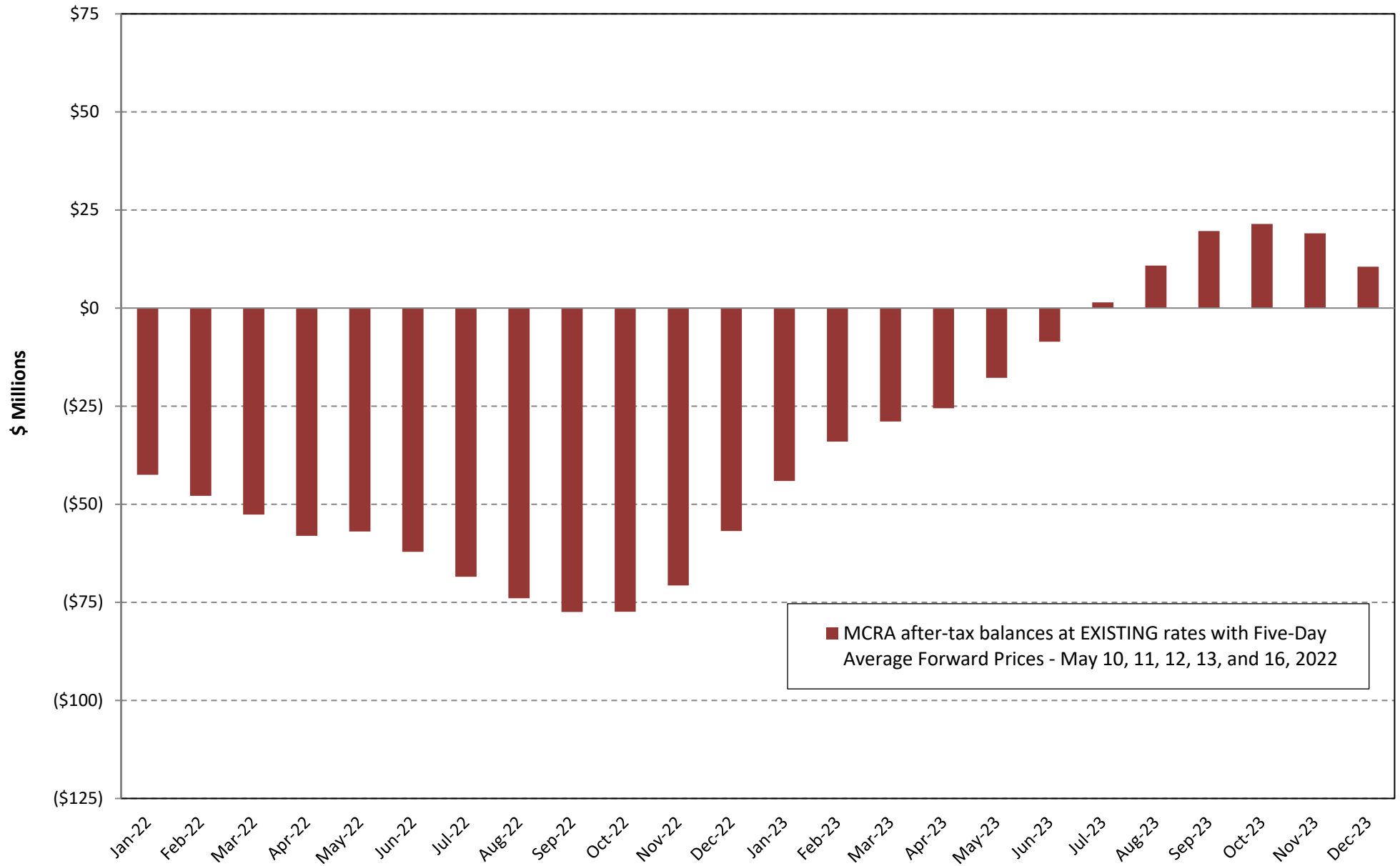
Slight differences in totals due to rounding.

FortisBC Energy Inc. - Mainland and Vancouver Island Service Area  
CCRA After-Tax Monthly Balances  
Recorded to April 2022 and Forecast to June 2024

Tab 1  
Page 9



FortisBC Energy Inc. - Mainland and Vancouver Island Service Area  
 MCRA After-Tax Monthly Balances  
 Recorded to April 2022 and Forecast to December 2023





**FORTISBC ENERGY INC. - MAINLAND AND VANCOUVER ISLAND SERVICE AREA**  
**CCRA INCURRED MONTHLY ACTIVITIES**  
**RECORDED PERIOD TO APR 2022 AND FORECAST TO JUN 2023**  
**FIVE-DAY AVERAGE FORWARD PRICES - MAY 10, 11, 12, 13, AND 16, 2022**

Tab 2  
Page 1

Line	(1)	(2)	(3)	(4)	(5)	(6)	(7)	(8)	(9)	(10)	(11)	(12)	(13)	(14)
														<b>Jan-22 to Jun-22 Total</b>
1		Recorded	Recorded	Recorded	Recorded	Projected	Projected							
2		Jan-22	Feb-22	Mar-22	Apr-22	May-22	Jun-22							
3	<b>CCRA QUANTITIES</b>													
4	Commodity Purchase (TJ)													
5	STN 2	9,805	8,858	9,813	9,489	9,766	9,451							57,182
6	AECO	3,094	2,806	3,108	3,039	3,147	3,045							18,239
7	Total Commodity Purchased	12,899	11,663	12,922	12,528	12,913	12,496							75,421
8	Fuel Gas Provided to Midstream	(488)	(441)	(488)	(441)	(488)	(472)							(2,816)
9	<b>Commodity Available for Sale</b>	12,412	11,223	12,434	12,088	12,425	12,024							72,605
10														
11	<b>CCRA COSTS</b>													
12	Commodity Costs (\$000)													
13	STN 2	\$ 46,843	\$ 40,217	\$ 45,071	\$ 50,444	\$ 55,967	\$ 63,687							\$ 302,229
14	AECO	13,098	12,552	14,409	16,413	18,458	20,816							95,746
15	Commodity Costs before Hedging	\$ 59,941	\$ 52,768	\$ 59,480	\$ 66,857	\$ 74,425	\$ 84,502							\$ 397,974
16	Hedging Cost / (Gain)	(4,743)	711	1,467	24	-	-							(2,541)
17	Core Market Administration Costs	127	136	113	96	139	139							750
18	<b>Total CCRA Costs</b>	\$ 55,323	\$ 53,647	\$ 61,064	\$ 66,959	\$ 74,565	\$ 84,642							\$ 396,183
19														
20														
21	CCRA Unit Cost (\$/GJ)	\$ 4.457	\$ 4.780	\$ 4.911	\$ 5.539	\$ 6.001	\$ 7.039							\$ 5.457
22														
23														
24		Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	<b>1-12 months Total</b>
25		Jul-22	Aug-22	Sep-22	Oct-22	Nov-22	Dec-22	Jan-23	Feb-23	Mar-23	Apr-23	May-23	Jun-23	
26	<b>CCRA QUANTITIES</b>													
27	Commodity Purchase (TJ)													
28	STN 2	9,766	9,766	9,451	9,766	9,451	9,766	9,766	8,821	9,766	9,451	9,766	9,451	114,986
29	AECO	3,147	3,147	3,045	3,147	3,045	3,147	3,147	2,842	3,147	3,045	3,147	3,045	37,049
30	Total Commodity Purchased	12,913	12,913	12,496	12,913	12,496	12,913	12,913	11,663	12,913	12,496	12,913	12,496	152,035
31	Fuel Gas Provided to Midstream	(488)	(488)	(472)	(488)	(472)	(488)	(488)	(440)	(488)	(472)	(488)	(472)	(5,742)
32	<b>Commodity Available for Sale</b>	12,425	12,425	12,024	12,425	12,024	12,425	12,425	11,222	12,425	12,024	12,425	12,024	146,293
33														
34	<b>CCRA COSTS</b>													
35	Commodity Costs (\$000)													
36	STN 2	\$ 66,943	\$ 64,324	\$ 62,180	\$ 64,404	\$ 71,026	\$ 77,827	\$ 78,478	\$ 69,853	\$ 63,863	\$ 39,948	\$ 38,301	\$ 37,555	\$ 734,701
37	AECO	21,571	20,860	20,263	21,017	22,394	24,332	24,429	21,863	20,408	13,335	12,870	12,585	235,928
38	Commodity Costs before Hedging	\$ 88,515	\$ 85,185	\$ 82,442	\$ 85,421	\$ 93,420	\$ 102,158	\$ 102,908	\$ 91,716	\$ 84,271	\$ 53,283	\$ 51,170	\$ 50,140	\$ 970,629
39	Hedging Cost / (Gain)	-	-	-	-	-	-	-	-	-	-	-	-	-
40	Core Market Administration Costs	139	139	139	139	139	139	139	139	139	139	139	139	1,673
41	<b>Total CCRA Costs</b>	\$ 88,654	\$ 85,324	\$ 82,582	\$ 85,560	\$ 93,559	\$ 102,298	\$ 103,047	\$ 91,856	\$ 84,411	\$ 53,422	\$ 51,310	\$ 50,279	\$ 972,302
42														
43														
44	CCRA Unit Cost (\$/GJ)	\$ 7.135	\$ 6.867	\$ 6.868	\$ 6.886	\$ 7.781	\$ 8.233	\$ 8.294	\$ 8.185	\$ 6.794	\$ 4.443	\$ 4.130	\$ 4.182	\$ 6.646

**FORTISBC ENERGY INC. - MAINLAND AND VANCOUVER ISLAND SERVICE AREA**  
**CCRA INCURRED MONTHLY ACTIVITIES**  
**FORECAST PERIOD FROM JUL 2023 TO JUN 2024**  
**FIVE-DAY AVERAGE FORWARD PRICES - MAY 10, 11, 12, 13, AND 16, 2022**

Tab 2  
Page 2

Line		(1)	(2)	(3)	(4)	(5)	(6)	(7)	(8)	(9)	(10)	(11)	(12)	(13)	(14)
1			Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	13-24 months
2			Jul-23	Aug-23	Sep-23	Oct-23	Nov-23	Dec-23	Jan-24	Feb-24	Mar-24	Apr-24	May-24	Jun-24	Total
3	CCRA QUANTITIES														
4	Commodity Purchase	(TJ)													
5	STN 2		9,856	9,856	9,538	9,856	9,538	9,856	9,856	9,220	9,856	9,538	9,856	9,538	116,359
6	AECO		3,175	3,175	3,073	3,175	3,073	3,175	3,175	2,971	3,175	3,073	3,175	3,073	37,491
7	Total Commodity Purchased		13,031	13,031	12,611	13,031	12,611	13,031	13,031	12,190	13,031	12,611	13,031	12,611	153,850
8	Fuel Gas Provided to Midstream		(492)	(492)	(476)	(492)	(476)	(492)	(492)	(460)	(492)	(476)	(492)	(476)	(5,811)
9	Commodity Available for Sale		12,539	12,539	12,134	12,539	12,134	12,539	12,539	11,730	12,539	12,134	12,539	12,134	148,039
10															
11															
12	CCRA COSTS	(\$000)													
13	Commodity Costs														
14	STN 2		\$ 39,064	\$ 39,083	\$ 36,713	\$ 38,549	\$ 42,338	\$ 47,082	\$ 48,072	\$ 44,553	\$ 43,296	\$ 32,004	\$ 31,455	\$ 29,963	\$ 472,172
15	AECO		12,932	12,918	12,138	12,767	13,574	15,100	15,419	14,290	13,880	10,697	10,533	10,040	154,288
16	Commodity Costs before Hedging		\$ 51,997	\$ 52,001	\$ 48,851	\$ 51,316	\$ 55,912	\$ 62,182	\$ 63,491	\$ 58,842	\$ 57,177	\$ 42,701	\$ 41,988	\$ 40,002	\$ 626,460
17	Hedging Cost / (Gain)		-	-	-	-	-	-	-	-	-	-	-	-	-
18	Core Market Administration Costs		139	139	139	139	139	139	139	139	139	139	139	139	1,673
19	Total CCRA Costs		\$ 52,136	\$ 52,140	\$ 48,991	\$ 51,455	\$ 56,052	\$ 62,321	\$ 63,630	\$ 58,982	\$ 57,316	\$ 42,841	\$ 42,127	\$ 40,142	\$ 628,133
20															
21															
22	CCRA Unit Cost	(\$/GJ)	\$ 4.158	\$ 4.158	\$ 4.037	\$ 4.104	\$ 4.619	\$ 4.970	\$ 5.075	\$ 5.028	\$ 4.571	\$ 3.531	\$ 3.360	\$ 3.308	\$ 4.243

Slight differences in totals due to rounding.

**FORTISBC ENERGY INC. - MAINLAND AND VANCOUVER ISLAND SERVICE AREA**  
**COMMODITY COST RECONCILIATION ACCOUNT (CCRA)**  
**COST OF GAS (COMMODITY COST RECOVERY CHARGE) FLOW-THROUGH**  
**FOR THE FORECAST PERIOD JUL 1, 2022 TO JUN 30, 2023**  
**FIVE-DAY AVERAGE FORWARD PRICES - MAY 10, 11, 12, 13, AND 16, 2022**

Tab 2  
Page 3

Line	Particulars	Unit	RS-1 to RS-7
	(1)		(2)
1	<b><u>CCRA Baseload</u></b>	TJ	146,293
2			
3			
4	<b><u>CCRA Incurred Costs</u></b>	\$000	
5	STN 2		\$ 734,701.0
6	AECO		235,928.1
7	CCRA Commodity Costs before Hedging		\$ 970,629.1
8	Hedging Cost / (Gain)		-
9	Core Market Administration Costs		1,672.5
10	<b>Total Incurred Costs before CCRA deferral amortization</b>		<b>\$ 972,301.6</b>
11			
12	Pre-tax CCRA Deficit / (Surplus) as of Jul 1, 2022		138,267.0
13	<b>Total CCRA Incurred Costs</b>		<b>\$ 1,110,568.6</b>
14			
15			
16	<b><u>CCRA Incurred Unit Costs</u></b>	\$/GJ	
17	CCRA Commodity Costs before Hedging		\$ 6.6348
18	Hedging Cost / (Gain)		-
19	Core Market Administration Costs		0.0114
20	Total Incurred Costs before CCRA deferral amortization		\$ 6.6463
21	Pre-tax CCRA Deficit / (Surplus) as of Jul 1, 2022		0.9451
22	<b>CCRA Gas Costs Incurred -- Flow-Through</b>		<b>\$ 7.5914</b>
23			
24			
25			
26			
27			
28			
29	<b><u>Cost of Gas (Commodity Cost Recovery Charge)</u></b>		<b>RS-1 to RS-7</b>
30			
31	<b>TESTED Flow-Through Cost of Gas effective Jul 1, 2022</b>		<b>\$ 7.591</b>
32			
33	Existing Cost of Gas (effective since Jan 1, 2022)		\$ 4.503
34			
35	<b>Cost of Gas Increase / (Decrease) -- Exceeded +/- \$1.00/GJ Cap</b>	\$/GJ	<b>\$ 3.088</b>
36			
37	Cost of Gas Percentage Increase / (Decrease)		68.58%

**FORTISBC ENERGY INC. - MAINLAND AND VANCOUVER ISLAND SERVICE AREA**  
**MCRA INCURRED MONTHLY ACTIVITIES FOR THE YEAR 2022**  
**FORECAST PERIODS WITH FIVE-DAY AVERAGE FORWARD PRICES - MAY 10, 11, 12, 13, AND 16, 2022**

Tab 2  
Page 4

Line	(1)	(2)	(3)	(4)	(5)	(6)	(7)	(8)	(9)	(10)	(11)	(12)	(13)	(14)	(15)
		Opening balance	Recorded Jan-22	Recorded Feb-22	Recorded Mar-22	Recorded Apr-22	Projected May-22	Projected Jun-22	Forecast Jul-22	Forecast Aug-22	Forecast Sep-22	Forecast Oct-22	Forecast Nov-22	Forecast Dec-22	<b>2022 Total</b>
1	<b>MCRA COSTS</b>	(\$000)													
2	<u>Midstream Commodity Related Costs</u>														
3	Propane Available for Sale - Purchase & Inventory Change		\$ 1,019.8	\$ 811.0	\$ 760.0	\$ 421.2	\$ 230.7	\$ 167.3	\$ 174.0	\$ 170.8	\$ 195.0	\$ 374.2	\$ 612.2	\$ 907.4	\$ 5,843.4
4	Propane Costs Recoveries via Commodity Rates <sup>(a)</sup>		(219.7)	(178.0)	(161.3)	(120.6)	(89.5)	(65.4)	(68.7)	(68.6)	(78.9)	(150.1)	(240.9)	(352.6)	(1,794.2)
5	Propane Own Use Recoveries <sup>(b)</sup>		(0.0)	(0.3)	(0.1)	(0.1)	-	-	-	-	-	-	-	-	(0.5)
6	Propane Costs to be Recovered via Midstream Rates		\$ 800.0	\$ 632.7	\$ 598.6	\$ 300.5	\$ 141.2	\$ 101.8	\$ 105.3	\$ 102.2	\$ 116.1	\$ 224.1	\$ 371.4	\$ 554.8	\$ 4,048.6
7	Midstream Natural Gas Costs before Hedging <sup>(c)</sup>		34,224.1	26,566.2	23,987.0	8,059.9	(132.4)	(151.4)	(158.9)	(150.2)	(145.9)	21,594.2	20,197.4	44,414.7	178,304.7
8	Hedging Cost / (Gain)		-	-	-	-	-	-	-	-	-	-	-	-	-
9	Imbalance <sup>(d)</sup>	\$ 2,592.3	(326.4)	(509.8)	181.8	298.8	-	-	-	-	-	-	-	(2,236.7)	(2,592.3)
10	Company Use Gas Recovered from O&M		(546.1)	(295.9)	(275.4)	(161.2)	(255.3)	(225.9)	(171.2)	(114.1)	(159.3)	(239.4)	(511.6)	(831.1)	(3,786.5)
11	Storage Withdrawal / (Injection) Activity <sup>(e)</sup>		17,930.0	15,237.3	11,381.0	2,251.6	(18,455.2)	(40,651.4)	(35,186.5)	(30,685.0)	(28,755.3)	(31,167.6)	24,938.5	37,445.4	(75,717.2)
12	Total Midstream Commodity Related Costs		<u>\$ 52,081.5</u>	<u>\$ 41,630.5</u>	<u>\$ 35,873.0</u>	<u>\$ 10,749.6</u>	<u>\$ (18,701.7)</u>	<u>\$ (40,926.9)</u>	<u>\$ (35,411.3)</u>	<u>\$ (30,847.1)</u>	<u>\$ (28,944.4)</u>	<u>\$ (9,588.7)</u>	<u>\$ 44,995.7</u>	<u>\$ 79,347.1</u>	<u>\$ 100,257.4</u>
13															
14	<u>Storage Related Costs</u>														
15	Storage Demand - Third Party Storage		\$ 2,656.8	\$ 2,623.7	\$ 2,630.2	\$ 2,571.2	\$ 3,884.0	\$ 4,023.8	\$ 4,024.5	\$ 4,024.2	\$ 4,014.6	\$ 4,020.5	\$ 2,652.7	\$ 2,671.4	\$ 39,797.6
16	On-System Storage - Mt. Hayes (LNG)		1,619.9	1,558.8	1,546.7	1,622.3	1,710.0	1,716.2	1,519.1	1,519.0	1,519.0	1,720.9	1,610.3	1,523.7	19,185.8
17	Total Storage Related Costs		<u>\$ 4,276.7</u>	<u>\$ 4,182.6</u>	<u>\$ 4,176.9</u>	<u>\$ 4,193.5</u>	<u>\$ 5,594.0</u>	<u>\$ 5,739.9</u>	<u>\$ 5,543.5</u>	<u>\$ 5,543.2</u>	<u>\$ 5,533.6</u>	<u>\$ 5,741.4</u>	<u>\$ 4,263.0</u>	<u>\$ 4,195.1</u>	<u>\$ 58,983.4</u>
18															
19	<u>Transportation Related Costs</u>														
20	Enbridge (BC Pipeline) - Westcoast Energy		\$ 16,644.3	\$ 16,585.0	\$ 14,661.6	\$ 14,015.4	\$ 13,308.0	\$ 13,269.2	\$ 13,301.0	\$ 13,416.8	\$ 13,345.0	\$ 13,574.2	\$ 15,798.2	\$ 16,020.6	\$ 173,939.3
21	TC energy (Foothills BC)		460.3	460.3	460.3	346.9	346.9	346.9	346.9	346.9	346.9	346.9	460.3	460.3	4,729.4
22	TC Energy (NOVA Alta)		1,011.4	1,015.0	1,013.2	946.7	1,013.2	1,013.2	1,013.2	1,013.2	1,013.2	1,013.2	1,013.2	1,013.2	12,091.7
23	Northwest Pipeline		710.9	690.5	717.3	356.0	371.6	386.8	390.4	371.0	357.9	361.5	731.0	761.8	6,206.6
24	FortisBC Huntingdon Inc.		11.5	11.5	11.5	11.5	11.5	11.5	11.5	11.5	11.5	11.5	11.5	11.5	138.5
25	Southern Crossing Pipeline		1,107.0	1,107.0	1,107.0	1,107.0	1,107.0	1,107.0	1,107.0	1,107.0	1,107.0	1,107.0	1,107.0	1,107.0	13,284.1
26	CNG Truck Bridge		120.6	-	-	-	-	-	-	-	-	-	-	-	120.6
27	Total Transportation Related Costs		<u>\$ 20,066.0</u>	<u>\$ 19,869.3</u>	<u>\$ 17,970.9</u>	<u>\$ 16,783.4</u>	<u>\$ 16,158.2</u>	<u>\$ 16,134.6</u>	<u>\$ 16,170.0</u>	<u>\$ 16,266.5</u>	<u>\$ 16,181.5</u>	<u>\$ 16,414.2</u>	<u>\$ 19,121.1</u>	<u>\$ 19,374.4</u>	<u>\$ 210,510.2</u>
28															
29	<u>Mitigation</u>														
30	Commodity Related Mitigation		\$ (9,622.6)	\$ (8,520.8)	\$ (21,320.2)	\$ (10,986.0)	\$ (12,973.6)	\$ (4,635.2)	\$ (20,410.5)	\$ (23,752.8)	\$ (14,904.7)	\$ (2,755.2)	\$ (15,980.7)	\$ (2,738.4)	\$ (148,600.5)
31	Storage Related Mitigation		2.0	(1,891.8)	(2,628.9)	1,872.7	-	-	-	-	-	-	-	-	(2,646.0)
32	Transportation Related Mitigation		(2,084.0)	(1,779.7)	(1,485.5)	(4,513.7)	(3,865.1)	(4,350.2)	(5,340.4)	(6,910.2)	(5,423.3)	(3,923.8)	(1,136.1)	(1,054.7)	(41,866.7)
33	Total Mitigation		<u>\$ (11,704.7)</u>	<u>\$ (12,192.3)</u>	<u>\$ (25,434.6)</u>	<u>\$ (13,627.0)</u>	<u>\$ (16,838.7)</u>	<u>\$ (8,985.4)</u>	<u>\$ (25,750.9)</u>	<u>\$ (30,663.0)</u>	<u>\$ (20,328.0)</u>	<u>\$ (6,679.0)</u>	<u>\$ (17,116.7)</u>	<u>\$ (3,793.1)</u>	<u>\$ (193,113.3)</u>
34															
35	<u>GSMIP Incentive Sharing</u>		<u>\$ 494.3</u>	<u>\$ 301.2</u>	<u>\$ 272.8</u>	<u>\$ 133.9</u>	<u>\$ 83.3</u>	<u>\$ 83.3</u>	<u>\$ 83.3</u>	<u>\$ 83.3</u>	<u>\$ 83.3</u>	<u>\$ 83.3</u>	<u>\$ 83.3</u>	<u>\$ 83.3</u>	<u>\$ 1,869.0</u>
36															
37	<u>Core Market Administration Costs</u>		<u>\$ 295.8</u>	<u>\$ 316.8</u>	<u>\$ 262.7</u>	<u>\$ 224.2</u>	<u>\$ 325.2</u>	<u>\$ 325.2</u>	<u>\$ 325.2</u>	<u>\$ 325.2</u>	<u>\$ 325.2</u>	<u>\$ 325.2</u>	<u>\$ 325.2</u>	<u>\$ 325.2</u>	<u>\$ 3,701.2</u>
38	<b>TOTAL MCRA COSTS</b>	(\$000)	<u>\$ 65,509.7</u>	<u>\$ 54,108.1</u>	<u>\$ 33,121.7</u>	<u>\$ 18,457.6</u>	<u>\$ (13,379.6)</u>	<u>\$ (27,629.2)</u>	<u>\$ (39,040.2)</u>	<u>\$ (39,291.8)</u>	<u>\$ (27,148.8)</u>	<u>\$ 6,296.5</u>	<u>\$ 51,671.7</u>	<u>\$ 99,532.1</u>	<u>\$ 182,207.8</u>
	(Line 12, 17, 27, 33, 35 & 37)														

Notes:

(a) Both propane costs and recoveries (commodity and midstream) for Revelstoke flow through in MCRA. That is, the propane costs to be recovered via midstream rates are net of the propane commodity recovery at the existing Commodity Cost Recovery Charge in Line 4.

(b) Facilities related own use gas (propane) is recovered via O&M; only the recorded is shown in the schedule. The own use gas used for vaporization and the UAF is recovered via the Storage and Transport rates.

(c) The total cost of UAF is included as a component of gas purchased. Sales UAF costs are recovered via gas cost recovery rates, while T-Service UAF costs are recovered via delivery revenues.

(d) Imbalance is composed of two components, Enbridge imbalance (difference between Enbridge metered and authorized deliveries) and Transportation imbalance (difference between the authorized receipts and customers' consumption or "burn").

The 2022 opening balance reflects FEI owed Enbridge / Transportation Marketers 638 TJ of gas valued at \$2,592K. As imbalance amounts can be either a debit or credit value, and typically remain within a narrow range, FEI does not forecast future imbalance amounts.

(e) The net impact to the MCRA related to the movement of commodity costs into or out of the Gas in Storage inventory account. Gas injections to storage result in credits to the MCRA, while withdrawals result in costs being debited to the MCRA.

Slight difference in totals due to rounding.

**FORTISBC ENERGY INC. - MAINLAND AND VANCOUVER ISLAND SERVICE AREA**  
**MCRA INCURRED MONTHLY ACTIVITIES FOR THE YEAR 2023**  
**FORECAST PERIODS WITH FIVE-DAY AVERAGE FORWARD PRICES - MAY 10, 11, 12, 13, AND 16, 2022**

Tab 2  
Page 5

Line	(1)	(2)	(3)	(4)	(5)	(6)	(7)	(8)	(9)	(10)	(11)	(12)	(13)	(14)	(15)
		Opening balance	Forecast Jan-23	Forecast Feb-23	Forecast Mar-23	Forecast Apr-23	Forecast May-23	Forecast Jun-23	Forecast Jul-23	Forecast Aug-23	Forecast Sep-23	Forecast Oct-23	Forecast Nov-23	Forecast Dec-23	<b>2023 Total</b>
1	<b>MCRA COSTS</b>	(\$000)													
2	<u>Midstream Commodity Related Costs</u>														
3	Propane Available for Sale - Purchase & Inventory Change		\$ 911.0	\$ 738.3	\$ 553.0	\$ 353.1	\$ 199.1	\$ 142.7	\$ 146.2	\$ 145.7	\$ 167.8	\$ 321.2	\$ 518.6	\$ 762.2	\$ 4,959.1
4	Propane Costs Recoveries via Commodity Rates <sup>(a)</sup>		(349.4)	(287.8)	(231.9)	(161.4)	(94.0)	(68.8)	(72.7)	(72.4)	(82.8)	(156.9)	(251.4)	(368.2)	(2,197.5)
5	Propane Own Use Recoveries <sup>(b)</sup>		-	-	-	-	-	-	-	-	-	-	-	-	-
6	Propane Costs to be Recovered via Midstream Rates		\$ 561.6	\$ 450.5	\$ 321.1	\$ 191.8	\$ 105.2	\$ 73.9	\$ 73.5	\$ 73.3	\$ 85.1	\$ 164.2	\$ 267.2	\$ 394.0	\$ 2,761.6
7	Midstream Natural Gas Costs before Hedging <sup>(c)</sup>		34,979.1	28,861.1	18,514.5	(98.1)	3,498.9	(92.4)	(560.4)	(559.8)	(525.5)	(553.0)	11,636.9	24,143.7	119,244.9
8	Hedging Cost / (Gain)		-	-	-	-	-	-	-	-	-	-	-	-	-
9	Imbalance <sup>(d)</sup>	\$ -	-	-	-	-	-	-	-	-	-	-	-	-	-
10	Company Use Gas Recovered from O&M		(967.7)	(762.6)	(650.4)	(455.4)	(255.3)	(225.9)	(171.2)	(114.1)	(159.3)	(239.4)	(511.6)	(831.1)	(5,344.0)
11	Storage Withdrawal / (Injection) Activity <sup>(e)</sup>		39,479.9	41,294.9	35,740.3	3,198.4	(24,655.1)	(23,891.9)	(21,349.7)	(18,216.3)	(16,150.5)	(6,541.5)	15,020.6	24,839.7	48,768.8
12	Total Midstream Commodity Related Costs		\$ 74,053.0	\$ 69,843.9	\$ 53,925.6	\$ 2,836.8	\$ (21,306.5)	\$ (24,136.4)	\$ (22,007.9)	\$ (18,816.8)	\$ (16,750.1)	\$ (7,169.6)	\$ 26,413.1	\$ 48,546.4	\$ 165,431.4
13															
14	<u>Storage Related Costs</u>														
15	Storage Demand - Third Party Storage		\$ 2,669.1	\$ 2,650.0	\$ 2,663.2	\$ 2,604.5	\$ 4,013.3	\$ 4,024.2	\$ 4,025.5	\$ 4,025.5	\$ 4,015.2	\$ 3,868.0	\$ 2,652.8	\$ 2,671.1	\$ 39,882.4
16	On-System Storage - Mt. Hayes (LNG)		1,523.8	1,523.7	1,523.3	1,519.0	1,710.0	1,716.2	1,519.1	1,519.0	1,519.0	1,720.9	1,610.3	1,523.7	18,927.8
17	Total Storage Related Costs		\$ 4,192.9	\$ 4,173.8	\$ 4,186.5	\$ 4,123.5	\$ 5,723.3	\$ 5,740.3	\$ 5,544.6	\$ 5,544.5	\$ 5,534.1	\$ 5,588.9	\$ 4,263.1	\$ 4,194.8	\$ 58,810.1
18															
19	<u>Transportation Related Costs</u>														
20	Enbridge (BC Pipeline) - Westcoast Energy		\$ 16,190.0	\$ 16,029.9	\$ 16,056.6	\$ 13,484.3	\$ 13,529.9	\$ 13,464.3	\$ 13,494.9	\$ 13,610.4	\$ 13,538.7	\$ 13,641.0	\$ 16,022.1	\$ 16,209.0	\$ 175,271.2
21	TC energy (Foothills BC)		502.9	502.9	502.9	379.0	379.0	379.0	379.0	379.0	379.0	379.0	502.9	502.9	5,167.8
22	TC Energy (NOVA Alta)		1,084.1	1,084.1	1,084.1	1,084.1	1,084.1	1,084.1	1,084.1	1,084.1	1,084.1	1,084.1	1,084.1	1,084.1	13,009.4
23	Northwest Pipeline		761.7	711.4	753.1	396.6	397.7	396.7	401.2	361.7	358.1	361.7	742.8	761.7	6,404.4
24	FortisBC Huntingdon Inc.		11.5	11.5	11.5	11.5	11.5	11.5	11.5	11.5	11.5	11.5	11.5	11.5	138.5
25	Southern Crossing Pipeline		1,107.0	1,107.0	1,107.0	1,107.0	1,107.0	1,107.0	1,107.0	1,107.0	1,107.0	1,107.0	1,107.0	1,107.0	13,284.1
26	Total Transportation Related Costs		\$ 19,657.3	\$ 19,446.8	\$ 19,515.3	\$ 16,462.7	\$ 16,509.3	\$ 16,442.7	\$ 16,477.8	\$ 16,553.8	\$ 16,478.6	\$ 16,584.4	\$ 19,470.4	\$ 19,676.3	\$ 213,275.3
27															
28	<u>Mitigation</u>														
29	Commodity Related Mitigation		\$ (1,715.3)	\$ (18,286.9)	\$ (32,334.3)	\$ (3,018.7)	\$ (1,730.1)	\$ (4,275.0)	\$ (11,955.0)	\$ (16,162.7)	\$ (10,751.8)	\$ (2,720.6)	\$ (9,893.8)	\$ (2,328.3)	\$ (115,172.6)
30	Storage Related Mitigation		-	-	-	-	-	-	-	-	-	-	-	-	-
31	Transportation Related Mitigation		(849.4)	(687.1)	(834.3)	(2,719.0)	(3,565.1)	(4,050.2)	(5,022.4)	(6,510.2)	(5,023.3)	(3,523.8)	(836.1)	(754.7)	(34,375.6)
32	Total Mitigation		\$ (2,564.8)	\$ (18,974.0)	\$ (33,168.6)	\$ (5,737.7)	\$ (5,295.2)	\$ (8,325.2)	\$ (16,977.4)	\$ (22,672.9)	\$ (15,775.1)	\$ (6,244.4)	\$ (10,729.9)	\$ (3,083.0)	\$ (149,548.2)
33															
34	<u>GSMIP Incentive Sharing</u>		\$ 83.3	\$ 83.3	\$ 83.3	\$ 83.3	\$ 83.3	\$ 83.3	\$ 83.3	\$ 83.3	\$ 83.3	\$ 83.3	\$ 83.3	\$ 83.3	\$ 1,000.0
35															
36	<u>Core Market Administration Costs</u>		\$ 325.2	\$ 325.2	\$ 325.2	\$ 325.2	\$ 325.2	\$ 325.2	\$ 325.2	\$ 325.2	\$ 325.2	\$ 325.2	\$ 325.2	\$ 325.2	\$ 3,902.5
37	<b>TOTAL MCRA COSTS</b>	(\$000)	\$ 95,747.0	\$ 74,899.0	\$ 44,867.3	\$ 18,093.7	\$ (3,960.5)	\$ (9,870.0)	\$ (16,554.4)	\$ (18,982.9)	\$ (10,104.1)	\$ 9,167.8	\$ 39,825.3	\$ 69,742.9	\$ 292,871.1
	(Line 12, 17, 26, 32, 34 & 36)														

Notes:

(a) Both propane costs and recoveries (commodity and midstream) for Revelstoke flow through in MCRA. That is, the propane costs to be recovered via midstream rates are net of the propane commodity recovery at the existing Commodity Cost Recovery Charge in Line 4.

(b) Facilities related own use gas (propane) is recovered via O&M; only the recorded is shown in the schedule. The own use gas used for vaporization and the UAF is recovered via the Storage and Transport rates.

(c) The total cost of UAF is included as a component of gas purchased. Sales UAF costs are recovered via gas cost recovery rates, while T-Service UAF costs are recovered via delivery revenues.

(d) Imbalance is composed of two components, Enbridge imbalance (difference between Enbridge metered and authorized deliveries) and Transportation imbalance (difference between the authorized receipts and customers' consumption or "burn").

(e) The net impact to the MCRA related to the movement of commodity costs into or out of the Gas in Storage inventory account. Gas injections to storage result in credits to the MCRA, while withdrawals result in costs being debited to the MCRA.

Slight difference in totals due to rounding.

**FORTISBC ENERGY INC. - MAINLAND AND VANCOUVER ISLAND SERVICE AREA**  
**STORAGE AND TRANSPORT RELATED CHARGES FLOW-THROUGH BY RATE SCHEDULE**  
**FOR THE FORECAST PERIOD JUL 2022 TO JUN 2023**  
**FIVE-DAY AVERAGE FORWARD PRICES - MAY 10, 11, 12, 13, AND 16, 2022**

Tab 2  
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Line	Particulars (1)	Unit						Total MCRA Gas Costs (7)	For Information Only			Term & Spot Gas Sales RS-14A (11)	Off-System Interruptible Sales RS-30 (12)
			Residential RS-1 (2)	Commercial RS-2 (3)	RS-3 (4)	General Firm RS-5 (5)	NGV RS-6 (6)		Seasonal RS-4 (8)	General Interruptible RS-7 (9)	LNG RS-46 (10)		
1	<b>MCRA Sales Quantity (Natural Gas &amp; Propane)</b>	TJ	81,867.2	29,095.4	25,079.9	20,829.5	20.2	156,892.2	161.3	6,646.8	4,650.3	-	18,611.6
2													
3	<b>Load Factor Adjusted Quantity</b>												
4	Load Factor <sup>(a)</sup>	%	31.3%	30.5%	35.9%	51.6%	100.0%						
5	Load Factor Adjusted Quantity	TJ	261,853.1	95,370.7	69,945.2	40,360.3	20.2	467,549.5					
6	Load Factor Adjusted Volumetric Allocation	%	56.0%	20.4%	15.0%	8.6%	0.0%	100.0%					
7													
8	<b>MCRA Cost of Gas - Load Factor Adjusted Allocation</b>												
9	Midstream Commodity Related Costs (Net of Mitigation)	\$000	\$ 17,190.7	\$ 6,261.1	\$ 4,591.9	\$ 2,649.7	\$ 1.3	\$ 30,694.8				\$ -	\$ -
10	Midstream Commodity (Natural Gas) Related Costs		94,884.0	34,558.1	25,345.0	14,624.8	7.3	169,419.3				-	120,961.0
11	Propane Costs to be Recovered via Midstream Rates		1,779.8	648.2	475.4	274.3	0.1	3,178.0				-	-
12	Midstream Commodity Related Mitigation		(79,473.1)	(28,945.2)	(21,228.5)	(12,249.4)	(6.1)	(141,902.5)				-	(120,961.0)
13	Storage Related Costs (Net of Mitigation)	\$000	33,020.8	12,026.6	8,820.4	5,089.6	2.6	58,960.0				-	-
14	Storage Related Costs		33,020.8	12,026.6	8,820.4	5,089.6	2.6	58,960.0				-	-
15	Storage Related Mitigation		-	-	-	-	-	-				-	-
16	Transportation Related Costs (Net of Mitigation)	\$000	98,047.7	35,710.4	26,190.1	15,112.4	7.6	175,068.2				-	-
17	Transportation Related Costs		118,486.1	43,154.3	31,649.5	18,262.6	9.2	211,561.8				-	-
18	Transportation Related Mitigation		(20,438.4)	(7,444.0)	(5,459.4)	(3,150.2)	(1.6)	(36,493.6)				-	-
19	GSMIP Incentive Sharing	\$000	560.1	204.0	149.6	86.3	0.0	1,000.0				-	-
20	Core Market Administration Costs - MCRA 70%	\$000	2,185.6	796.0	583.8	336.9	0.2	3,902.5				-	-
21	<b>Total Midstream Cost of Gas Allocated by Rate Class</b>	\$000	\$ 151,004.9	\$ 54,998.2	\$ 40,335.8	\$ 23,274.9	\$ 11.7	\$ 269,625.5				\$ -	\$ -
22	T-Service UAF to be recovered via delivery revenues <sup>(b)</sup>							2,170.4					
23	<b>Total MCRA Gas Costs <sup>(c)</sup></b>							\$ 271,795.9					
24	<b>1/2 of Pre-Tax Amort. MCRA Deficit/(Surplus) as of Jul 1, 2022</b>	\$000	\$ (23,807.5)	\$ (8,671.0)	\$ (6,359.4)	\$ (3,669.5)	\$ (1.8)	\$ (42,509.3)					
25													
26													
27	<b>MCRA Cost of Gas Unitized</b>							<b>Average Costs</b>					
28	Midstream Commodity Related Costs (Net of Mitigation)	\$/GJ	\$ 0.2100	\$ 0.2152	\$ 0.1831	\$ 0.1272	\$ 0.0656	\$ 0.1956					
29	Storage Related Costs (Net of Mitigation)	\$/GJ	0.4033	0.4134	0.3517	0.2443	0.1261	0.3758					
30	Transportation Related Costs (Net of Mitigation)	\$/GJ	1.1976	1.2274	1.0443	0.7255	0.3744	1.1159					
31	GSMIP Incentive Sharing	\$/GJ	0.0068	0.0070	0.0060	0.0041	0.0021	0.0064					
32	Core Market Administration Costs - MCRA 70%	\$/GJ	0.0267	0.0274	0.0233	0.0162	0.0083	0.0249					
33	<b>MCRA Flow-Through Costs before MCRA deferral amortization</b>	\$/GJ	\$ 1.8445	\$ 1.8903	\$ 1.6083	\$ 1.1174	\$ 0.5766	\$ 1.7185					
34	<b>MCRA Deferral Amortization via Rate Rider 6</b>	\$/GJ	\$ (0.2908)	\$ (0.2980)	\$ (0.2536)	\$ (0.1762)	\$ (0.0909)	\$ (0.2709)					

**Notes:**

(a) Based on the historical 3-year (2018, 2019, and 2020 data) rolling average load factors for Rate Schedules 1, 2, 3 and 5.

(b) The total cost of UAF (Sales Rate Classes and T-Service) is included as a component of gas purchased. Sales UAF costs are recovered via gas cost recovery rates.

As the T-Service UAF costs are recovered via delivery revenues, they are excluded from the storage and transportation flow-through calculation.

(c) Reconciled to the Total MCRA Costs on Tab 1, Page 7, Col. 3, Line 48, with monthly breakdown on Tab 2, Page 6.1.

(d) Storage & Transport and MCRA Rate Rider 6 charges for RS-4, RS-6P-E360S, RS-7, and RS-46 are set at the RS-5 tariff rates. For midstream cost allocation purposes the RS-5 allocations include RS-4, RS-5, RS-6P-E360S, RS-7, and RS-46 forecast sales.

Slight differences in totals due to rounding.

**FORTISBC ENERGY INC. - MAINLAND AND VANCOUVER ISLAND SERVICE AREA**  
**MCRA INCURRED MONTHLY ACTIVITIES FOR THE PERIOD FROM JUL 2022 TO JUN 2023**  
**FORECAST PERIODS WITH FIVE-DAY AVERAGE FORWARD PRICES - MAY 10, 11, 12, 13, AND 16, 2022**

Tab 2  
Page 6.1

Line	(1)	(2)	(3)	(4)	(5)	(6)	(7)	(8)	(9)	(10)	(11)	(12)	(13)	(14)	(15)
		Opening balance	Forecast Jul-22	Forecast Aug-22	Forecast Sep-22	Forecast Oct-22	Forecast Nov-22	Forecast Dec-22	Forecast Jan-23	Forecast Feb-23	Forecast Mar-23	Forecast Apr-23	Forecast May-23	Forecast Jun-23	Jul-22 to Jun-23 Total
1	MCRA COSTS	(\$000)													
2	Midstream Commodity Related Costs														
3	Propane Available for Sale - Purchase & Inventory Change		\$ 174.0	\$ 170.8	\$ 195.0	\$ 374.2	\$ 612.2	\$ 907.4	\$ 911.0	\$ 738.3	\$ 553.0	\$ 353.1	\$ 199.1	\$ 142.7	\$ 5,330.8
4	Propane Costs Recoveries via Commodity Rates <sup>(a)</sup>		(68.7)	(68.6)	(78.9)	(150.1)	(240.9)	(352.6)	(349.4)	(287.8)	(231.9)	(161.4)	(94.0)	(68.8)	(2,152.9)
5	Propane Own Use Recoveries <sup>(b)</sup>		-	-	-	-	-	-	-	-	-	-	-	-	-
6	Propane Costs to be Recovered via Midstream Rates		\$ 105.3	\$ 102.2	\$ 116.1	\$ 224.1	\$ 371.4	\$ 554.8	\$ 561.6	\$ 450.5	\$ 321.1	\$ 191.8	\$ 105.2	\$ 73.9	\$ 3,178.0
7	Midstream Natural Gas Costs before Hedging <sup>(c)</sup>		(158.9)	(150.2)	(145.9)	21,594.2	20,197.4	44,414.7	34,979.1	28,861.1	18,514.5	(98.1)	3,498.9	(92.4)	171,414.4
8	Hedging Cost / (Gain)		-	-	-	-	-	-	-	-	-	-	-	-	-
9	Imbalance <sup>(d)</sup>		-	-	-	-	-	(2,236.7)	-	-	-	-	-	-	(2,236.7)
10	Company Use Gas Recovered from O&M		(171.2)	(114.1)	(159.3)	(239.4)	(511.6)	(831.1)	(967.7)	(762.6)	(650.4)	(455.4)	(255.3)	(225.9)	(5,344.0)
11	Storage Withdrawal / (Injection) Activity <sup>(e)</sup>		(35,186.5)	(30,685.0)	(28,755.3)	(31,167.6)	24,938.5	37,445.4	39,479.9	41,294.9	35,740.3	3,198.4	(24,655.1)	(23,891.9)	7,755.9
12	Total Midstream Commodity Related Costs		\$ (35,411.3)	\$ (30,847.1)	\$ (28,944.4)	\$ (9,588.7)	\$ 44,995.7	\$ 79,347.1	\$ 74,053.0	\$ 69,843.9	\$ 53,925.6	\$ 2,836.8	\$ (21,306.5)	\$ (24,136.4)	\$ 174,767.7
13															
14	Storage Related Costs														
15	Storage Demand - Third Party Storage		\$ 4,024.5	\$ 4,024.2	\$ 4,014.6	\$ 4,020.5	\$ 2,652.7	\$ 2,671.4	\$ 2,669.1	\$ 2,650.0	\$ 2,663.2	\$ 2,604.5	\$ 4,013.3	\$ 4,024.2	\$ 40,032.2
16	On-System Storage - Mt. Hayes (LNG)		1,519.1	1,519.0	1,519.0	1,720.9	1,610.3	1,523.7	1,523.8	1,523.7	1,523.3	1,519.0	1,710.0	1,716.2	18,927.8
17	Total Storage Related Costs		\$ 5,543.5	\$ 5,543.2	\$ 5,533.6	\$ 5,741.4	\$ 4,263.0	\$ 4,195.1	\$ 4,192.9	\$ 4,173.8	\$ 4,186.5	\$ 4,123.5	\$ 5,723.3	\$ 5,740.3	\$ 58,960.0
18															
19	Transportation Related Costs														
20	Enbridge (BC Pipeline) - Westcoast Energy		\$ 13,301.0	\$ 13,416.8	\$ 13,345.0	\$ 13,574.2	\$ 15,798.2	\$ 16,020.6	\$ 16,190.0	\$ 16,029.9	\$ 16,056.6	\$ 13,484.3	\$ 13,529.9	\$ 13,464.3	\$ 174,210.9
21	TC energy (Foothills BC)		346.9	346.9	346.9	346.9	460.3	460.3	502.9	502.9	502.9	379.0	379.0	379.0	4,953.9
22	TC Energy (NOVA Alta)		1,013.2	1,013.2	1,013.2	1,013.2	1,013.2	1,013.2	1,084.1	1,084.1	1,084.1	1,084.1	1,084.1	1,084.1	12,583.8
23	Northwest Pipeline		390.4	371.0	357.9	361.5	731.0	761.8	761.7	711.4	753.1	396.6	397.7	396.7	6,390.7
24	FortisBC Huntingdon Inc.		11.5	11.5	11.5	11.5	11.5	11.5	11.5	11.5	11.5	11.5	11.5	11.5	138.5
25	Southern Crossing Pipeline		1,107.0	1,107.0	1,107.0	1,107.0	1,107.0	1,107.0	1,107.0	1,107.0	1,107.0	1,107.0	1,107.0	1,107.0	13,284.1
26	Total Transportation Related Costs		\$ 16,170.0	\$ 16,266.5	\$ 16,181.5	\$ 16,414.2	\$ 19,121.1	\$ 19,374.4	\$ 19,657.3	\$ 19,446.8	\$ 19,515.3	\$ 16,462.7	\$ 16,509.3	\$ 16,442.7	\$ 211,561.8
27															
28	Mitigation														
29	Commodity Related Mitigation		\$ (20,410.5)	\$ (23,752.8)	\$ (14,904.7)	\$ (2,755.2)	\$ (15,980.7)	\$ (2,738.4)	\$ (1,715.3)	\$ (18,286.9)	\$ (32,334.3)	\$ (3,018.7)	\$ (1,730.1)	\$ (4,275.0)	\$ (141,902.5)
30	Storage Related Mitigation		-	-	-	-	-	-	-	-	-	-	-	-	-
31	Transportation Related Mitigation		(5,340.4)	(6,910.2)	(5,423.3)	(3,923.8)	(1,136.1)	(1,054.7)	(849.4)	(687.1)	(834.3)	(2,719.0)	(3,565.1)	(4,050.2)	(36,493.6)
32	Total Mitigation		\$ (25,750.9)	\$ (30,663.0)	\$ (20,328.0)	\$ (6,679.0)	\$ (17,116.7)	\$ (3,793.1)	\$ (2,564.8)	\$ (18,974.0)	\$ (33,168.6)	\$ (5,737.7)	\$ (5,295.2)	\$ (8,325.2)	\$ (178,396.1)
33															
34	GSMIP Incentive Sharing		\$ 83.3	\$ 83.3	\$ 83.3	\$ 83.3	\$ 83.3	\$ 83.3	\$ 83.3	\$ 83.3	\$ 83.3	\$ 83.3	\$ 83.3	\$ 83.3	\$ 1,000.0
35															
36	Core Market Administration Costs		\$ 325.2	\$ 325.2	\$ 325.2	\$ 325.2	\$ 325.2	\$ 325.2	\$ 325.2	\$ 325.2	\$ 325.2	\$ 325.2	\$ 325.2	\$ 325.2	\$ 3,902.5
37	TOTAL MCRA COSTS	(\$000)	\$(39,040.2)	\$(39,291.8)	\$(27,148.8)	\$ 6,296.5	\$ 51,671.7	\$ 99,532.1	\$ 95,747.0	\$ 74,899.0	\$ 44,867.3	\$ 18,093.7	\$ (3,960.5)	\$ (9,870.0)	\$ 271,795.9
	(Line 12, 17, 26, 32, 34 & 36)														

Notes:

- (a) Both propane costs and recoveries (commodity and midstream) for Revelstoke flow through in MCRA. That is, the propane costs to be recovered via midstream rates are net of the propane commodity recovery at the existing Commodity Cost Recovery Charge in Line 4.
- (b) Facilities related own use gas (propane) is recovered via O&M; only the recorded is shown in the schedule. The own use gas used for vaporization and the UAF is recovered via the Storage and Transport rates.
- (c) The total cost of UAF is included as a component of gas purchased. Sales UAF costs are recovered via gas cost recovery rates, while T-Service UAF costs are recovered via delivery revenues.
- (d) Imbalance is composed of two components, Enbridge imbalance (difference between Enbridge metered and authorized deliveries) and Transportation imbalance (difference between the authorized receipts and customers' consumption or "burn"). For developing the forecast midstream costs opening imbalance amounts are forecast to be settled at the beginning of the prospective period. As imbalance amounts can be either a debit or credit value, and typically remain within a narrow range, FEI does not forecast future imbalance amounts.
- (e) The net impact to the MCRA related to the movement of commodity costs into or out of the Gas in Storage inventory account. Gas injections to storage result in credits to the MCRA, while withdrawals result in costs being debited to the MCRA.

Slight difference in totals due to rounding.

**FORTISBC ENERGY INC. - MAINLAND AND VANCOUVER ISLAND SERVICE AREA**  
**CCRA MONTHLY BALANCES AT TESTED RATES (AFTER ADJUSTMENTS FOR ENERGY DIFFERENCES)**  
**FOR THE FORECAST PERIOD FROM JUL 2022 TO JUN 2024**  
**FIVE-DAY AVERAGE FORWARD PRICES - May 10, 11, 12, 13, AND 16, 2022**  
**\$(Millions)**

Tab 3

Page 1

Line	(1)	(2)	(3)	(4)	(5)	(6)	(7)	(8)	(9)	(10)	(11)	(12)	(13)	(14)
1		Recorded	Recorded	Recorded	Recorded	Projected	Projected							<b>Jan-22</b>
2		Jan-22	Feb-22	Mar-22	Apr-22	May-22	Jun-22							<b>to</b>
														<b>Jun-22</b>
3	CCRA Balance - Beginning (Pre-tax) <sup>(a)</sup>	\$ 68	\$ 68	\$ 71	\$ 76	\$ 89	\$ 107							<b>\$ 68</b>
4	Gas Costs Incurred	55	54	61	67	75	85							396
5	Revenue from <b>APPROVED</b> Recovery Rate	(56)	(51)	(56)	(54)	(56)	(54)							(327)
6	<b>CCRA Balance - Ending (Pre-tax) <sup>(b)</sup></b>	<b>\$ 68</b>	<b>\$ 71</b>	<b>\$ 76</b>	<b>\$ 89</b>	<b>\$ 107</b>	<b>\$ 138</b>							<b>\$ 138</b>
7														
8	Tax Rate	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%							27.0%
9														
10	<b>CCRA Balance - Ending (After-tax) <sup>(c)</sup></b>	<b>\$ 50</b>	<b>\$ 52</b>	<b>\$ 56</b>	<b>\$ 65</b>	<b>\$ 78</b>	<b>\$ 101</b>							<b>\$ 101</b>
11														
12														
13														<b>Jul-22</b>
14		Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	<b>to</b>
15		Jul-22	Aug-22	Sep-22	Oct-22	Nov-22	Dec-22	Jan-23	Feb-23	Mar-23	Apr-23	May-23	Jun-23	<b>Jun-23</b>
16	CCRA Balance - Beginning (Pre-tax) <sup>(a)</sup>	\$ 138	\$ 133	\$ 124	\$ 115	\$ 106	\$ 108	\$ 116	\$ 125	\$ 132	\$ 122	\$ 84	\$ 41	<b>\$ 138</b>
17	Gas Costs Incurred	89	85	83	86	94	102	103	92	84	53	51	50	972
18	Revenue from <b>TESTED</b> Recovery Rates	(94)	(94)	(91)	(94)	(91)	(94)	(94)	(85)	(94)	(91)	(94)	(91)	(1,111)
19	<b>CCRA Balance - Ending (Pre-tax) <sup>(b)</sup></b>	<b>\$ 133</b>	<b>\$ 124</b>	<b>\$ 115</b>	<b>\$ 106</b>	<b>\$ 108</b>	<b>\$ 116</b>	<b>\$ 125</b>	<b>\$ 132</b>	<b>\$ 122</b>	<b>\$ 84</b>	<b>\$ 41</b>	<b>\$ 0</b>	<b>\$ 0</b>
20														
21	Tax Rate	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%
22														
23	<b>CCRA Balance - Ending (After-tax) <sup>(c)</sup></b>	<b>\$ 97</b>	<b>\$ 90</b>	<b>\$ 84</b>	<b>\$ 77</b>	<b>\$ 79</b>	<b>\$ 85</b>	<b>\$ 91</b>	<b>\$ 96</b>	<b>\$ 89</b>	<b>\$ 61</b>	<b>\$ 30</b>	<b>\$ 0</b>	<b>\$ 0</b>
24														
25														
26														<b>Jul-23</b>
27		Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	<b>to</b>
28		Jul-23	Aug-23	Sep-23	Oct-23	Nov-23	Dec-23	Jan-24	Feb-24	Mar-24	Apr-24	May-24	Jun-24	<b>Jun-24</b>
29	CCRA Balance - Beginning (Pre-tax) <sup>(a)</sup>	\$ 0	\$ (43)	\$ (86)	\$ (129)	\$ (173)	\$ (209)	\$ (242)	\$ (273)	\$ (303)	\$ (341)	\$ (391)	\$ (444)	<b>\$ 0</b>
30	Gas Costs Incurred	52	52	49	51	56	62	64	59	57	43	42	40	628
31	Revenue from <b>TESTED</b> Recovery Rates	(95)	(95)	(92)	(95)	(92)	(95)	(95)	(89)	(95)	(92)	(95)	(92)	(1,124)
32	<b>CCRA Balance - Ending (Pre-tax) <sup>(b)</sup></b>	<b>\$ (43)</b>	<b>\$ (86)</b>	<b>\$ (129)</b>	<b>\$ (173)</b>	<b>\$ (209)</b>	<b>\$ (242)</b>	<b>\$ (273)</b>	<b>\$ (303)</b>	<b>\$ (341)</b>	<b>\$ (391)</b>	<b>\$ (444)</b>	<b>\$ (496)</b>	<b>\$ (496)</b>
33														
34	Tax Rate	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%
35														
36	<b>CCRA Balance - Ending (After-tax) <sup>(c)</sup></b>	<b>\$ (31)</b>	<b>\$ (63)</b>	<b>\$ (94)</b>	<b>\$ (126)</b>	<b>\$ (153)</b>	<b>\$ (177)</b>	<b>\$ (200)</b>	<b>\$ (221)</b>	<b>\$ (249)</b>	<b>\$ (285)</b>	<b>\$ (324)</b>	<b>\$ (362)</b>	<b>\$ (362)</b>

Notes:

(a) Pre-tax opening balances are restated based on current income tax rates, to reflect grossed-up after tax amounts.

(b) For rate setting purposes CCRA pre-tax balances include grossed-up projected deferred interest of approximately \$0.5 million as at June 30, 2022.

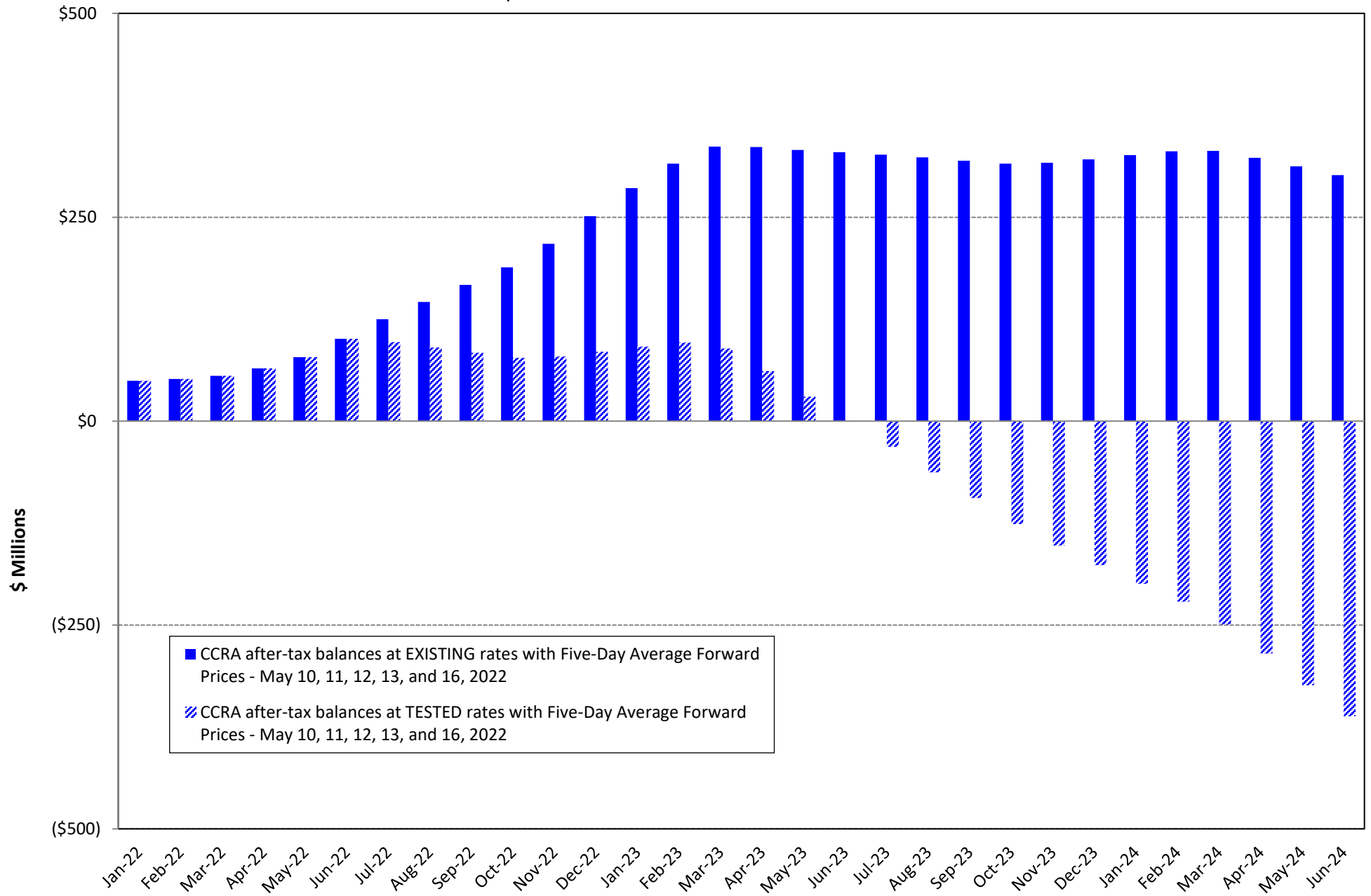
(c) For rate setting purposes CCRA after tax balances are independently grossed-up to reflect pre-tax amounts.

Slight differences in totals due to rounding.



FortisBC Energy Inc. - Mainland and Vancouver Island Service Area  
 CCRA After-Tax Monthly Balances  
 Recorded to April 2022 and Forecast June 2024

Tab 3  
 Page 2



**FORTISBC ENERGY INC. - MAINLAND AND VANCOUVER ISLAND SERVICE AREA**  
**MCRA MONTHLY BALANCES AT TESTED COMMODITY COST RECOVERY CHARGE, STORAGE AND TRANSPORT CHARGES (AFTER ADJUSTMENTS FOR ENERGY DIFFERENCES)**  
**FOR THE FORECAST PERIOD FROM JUL 2022 TO DEC 2024**  
**FIVE-DAY AVERAGE FORWARD PRICES - MAY 10, 11, 12, 13, AND 16, 2022**  
**\$(Millions)**

Tab 3  
Page 3

Line	(1)	(2)	(3)	(4)	(5)	(6)	(7)	(8)	(9)	(10)	(11)	(12)	(13)	(14)
1														
2		Recorded	Recorded	Recorded	Recorded	Projected	Projected	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	<b>Total</b>
3		Jan-22	Feb-22	Mar-22	Apr-22	May-22	Jun-22	Jul-22	Aug-22	Sep-22	Oct-22	Nov-22	Dec-22	<b>2022</b>
4	MCRA Balance - Beginning (Pre-tax) <sup>(a)</sup>	\$ (40)	\$ (58)	\$ (66)	\$ (72)	\$ (80)	\$ (78)	\$ (85)	\$ (71)	\$ (55)	\$ (41)	\$ (37)	\$ (43)	\$ (40)
5	<b>2022 MCRA Activities</b>													
6	<u>Rate Rider 6</u>													
7	<i>Approved Amount to be amortized in 2022</i>													\$ (22)
8	<i>Rider 6 Amortization at APPROVED 2022 Rates</i>	\$ 3	\$ 3	\$ 2	\$ 2	\$ 1	\$ 1	\$ 1	\$ 1	\$ 1	\$ 2	\$ 3	\$ 4	\$ 22
9	<u>Midstream Base Rates</u>													
10	<i>Gas Costs Incurred</i>	\$ 66	\$ 54	\$ 33	\$ 18	\$ (13)	\$ (28)	\$ (40)	\$ (40)	\$ (29)	\$ 5	\$ 50	\$ 98	\$ 174
11	<i>Revenue from <b>APPROVED</b> Recovery Rates</i>	(86)	(64)	(42)	(28)	14	22	54	56	41	(2)	(58)	(116)	(211)
12	Total Midstream Base Rates (Pre-tax)	\$ (21)	\$ (10)	\$ (9)	\$ (9)	\$ 0	\$ (6)	\$ 14	\$ 16	\$ 13	\$ 2	\$ (8)	\$ (18)	\$ (36)
13														
14	<b>MCRA Cumulative Balance - Ending (Pre-tax) <sup>(b)</sup></b>	\$ (58)	\$ (66)	\$ (72)	\$ (80)	\$ (78)	\$ (85)	\$ (71)	\$ (55)	\$ (41)	\$ (37)	\$ (43)	\$ (57)	\$ (57)
15	Tax Rate	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%
16														
17	<b>MCRA Cumulative Balance - Ending (After-tax) <sup>(c)</sup></b>	\$ (42)	\$ (48)	\$ (53)	\$ (58)	\$ (57)	\$ (62)	\$ (52)	\$ (40)	\$ (30)	\$ (27)	\$ (31)	\$ (42)	\$ (42)
18														
19		Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	<b>Total</b>
20		Jan-23	Feb-23	Mar-23	Apr-23	May-23	Jun-23	Jul-23	Aug-23	Sep-23	Oct-23	Nov-23	Dec-23	<b>2023</b>
21	MCRA Balance - Beginning (Pre-tax) <sup>(a)</sup>	\$ (57)	\$ (72)	\$ (83)	\$ (88)	\$ (83)	\$ (57)	\$ (26)	\$ 11	\$ 47	\$ 78	\$ 84	\$ 66	\$ (57)
22	<b>2023 MCRA Activities</b>													
23	<u>Rate Rider 6</u>													
24														
25	<i>Rider 6 Amortization at APPROVED 2022 Rates</i>	\$ 3	\$ 3	\$ 2	\$ 2	\$ 1	\$ 1	\$ 1	\$ 1	\$ 1	\$ 2	\$ 3	\$ 4	\$ 23
26	<u>Midstream Base Rates</u>													
27	<i>Gas Costs Incurred</i>	\$ 95	\$ 74	\$ 44	\$ 17	\$ (5)	\$ (11)	\$ (18)	\$ (20)	\$ (12)	\$ 8	\$ 38	\$ 68	\$ 279
28	<i>Revenue from <b>2022</b> Recovery Rates</i>	(113)	(87)	(51)	(14)	29	41	54	56	41	(3)	(59)	(118)	(224)
29	Total Midstream Base Rates (Pre-tax)	\$ (18)	\$ (14)	\$ (7)	\$ 3	\$ 24	\$ 30	\$ 36	\$ 36	\$ 29	\$ 5	\$ (21)	\$ (49)	\$ 55
30														
31	<b>MCRA Cumulative Balance - Ending (Pre-tax) <sup>(b)</sup></b>	\$ (72)	\$ (83)	\$ (88)	\$ (83)	\$ (57)	\$ (26)	\$ 11	\$ 47	\$ 78	\$ 84	\$ 66	\$ 20	\$ 20
32	Tax Rate	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%
33														
34	<b>MCRA Cumulative Balance - Ending (After-tax) <sup>(c)</sup></b>	\$ (53)	\$ (61)	\$ (64)	\$ (60)	\$ (42)	\$ (19)	\$ 8	\$ 35	\$ 57	\$ 61	\$ 48	\$ 15	\$ 15

Notes:

- (a) Pre-tax opening balances are restated based on current income tax rates, to reflect grossed-up after tax amounts.  
(b) For rate setting purposes MCRA pre-tax balances include grossed-up projected deferred interest of approximately \$2.3 million credit as at June 30, 2022.  
(c) For rate setting purposes MCRA after-tax balances are independently grossed-up to reflect pre-tax amounts.

Slight differences in totals due to rounding.

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SCHEDULE 1

RATE SCHEDULE 1: RESIDENTIAL SERVICE		EXISTING RATES JANUARY 1, 2022	COMMODITY RELATED CHARGES CHANGES	TESTED JULY 1, 2022 RATES
Line No.	Particulars	Mainland and Vancouver Island	Mainland and Vancouver Island	Mainland and Vancouver Island
	(1)	(2)	(3)	(4)
1	<u>Delivery Margin Related Charges</u>			
2	<b>Basic Charge per Day</b>	<b>\$0.4085</b>	<b>\$0.0000</b>	<b>\$0.4085</b>
3	Rider 2 Clean Growth Innovation Fund Rate Rider per Day	\$0.0131	\$0.000	\$0.0131
4	Subtotal of per Day <b>Delivery Margin Related Charges</b>	<b>\$0.4216</b>	<b>\$0.000</b>	<b>\$0.4216</b>
5				
6				
7	Delivery Charge per GJ	\$5.455	\$0.000	\$5.455
8	Rider 3 BVA Rate Rider per GJ	\$0.059	\$0.000	\$0.059
9	Rider 5 RSAM per GJ	\$0.012	\$0.000	\$0.012
10	Subtotal of Per GJ <b>Delivery Margin Related Charges</b>	<b>\$5.526</b>	<b>\$0.000</b>	<b>\$5.526</b>
11				
12				
13	<u>Commodity Related Charges</u>			
14	Storage and Transport Charge per GJ	\$1.505	\$0.000	\$1.505
15	Rider 6 MCRA per GJ	(\$0.154 )	\$0.000	(\$0.154 )
18	Subtotal <b>Storage and Transport Related Charges per GJ</b>	<b>\$1.351</b>	<b>\$0.000</b>	<b>\$1.351</b>
19				
20	<b>Cost of Gas (Commodity Cost Recovery Charge) per GJ</b>	<b>\$4.503</b>	<b>\$3.088</b>	<b>\$7.591</b>

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SCHEDULE 1B

RATE SCHEDULE 1B: RESIDENTIAL BIOMETHANE SERVICE		EXISTING RATES JANUARY 1, 2022	COMMODITY RELATED CHARGES CHANGES	TESTED JULY 1, 2022 RATES
Line No.	Particulars	Mainland and Vancouver Island	Mainland and Vancouver Island	Mainland and Vancouver Island
	(1)	(2)	(3)	(4)
1	<u>Delivery Margin Related Charges</u>			
2	<b>Basic Charge per Day</b>	<b>\$0.4085</b>	<b>\$0.0000</b>	<b>\$0.4085</b>
3	Rider 2 Clean Growth Innovation Fund Rate Rider per Day	\$0.0131	\$0.000	\$0.0131
4	Subtotal of per Day <b>Delivery Margin Related Charges</b>	<b>\$0.4216</b>	<b>\$0.000</b>	<b>\$0.4216</b>
5				
6	Delivery Charge per GJ	\$5.455	\$0.000	\$5.455
7	Rider 3 BVA Rate Rider per GJ	\$0.059	\$0.000	\$0.059
8	Rider 5 RSAM per GJ	\$0.012	\$0.000	\$0.012
9	Subtotal of Per GJ <b>Delivery Margin Related Charges</b>	<b>\$5.526</b>	<b>\$0.000</b>	<b>\$5.526</b>
10				
11				
12	<u>Commodity Related Charges</u>			
13	Storage and Transport Charge per GJ	\$1.505	\$0.000	\$1.505
14	Rider 6 MCRA per GJ	(\$0.154 )	\$0.000	(\$0.154 )
17	Subtotal <b>Storage and Transport Related Charges per GJ</b>	<b>\$1.351</b>	<b>\$0.000</b>	<b>\$1.351</b>
18				
19				
20	<b>Cost of Gas (Commodity Cost Recovery Charge) per GJ</b>	<b>\$4.503</b>	<b>\$3.088</b>	<b>\$7.591</b>
21				
22	<b>Cost of Biomethane per GJ</b>	<b>\$13.808</b>	<b>\$0.000</b>	<b>\$13.808</b>
23	(Biomethane Energy Recovery Charge)			

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SCHEDULE 2

RATE SCHEDULE 2: SMALL COMMERCIAL SERVICE		EXISTING RATES JANUARY 1, 2022	COMMODITY RELATED CHARGES CHANGES	TESTED JULY 1, 2022 RATES
Line No.	Particulars	Mainland and Vancouver Island	Mainland and Vancouver Island	Mainland and Vancouver Island
	(1)	(2)	(3)	(4)
1	<u>Delivery Margin Related Charges</u>			
2	<b>Basic Charge per Day</b>	<b>\$0.9485</b>	<b>\$0.0000</b>	<b>\$0.9485</b>
3	Rider 2 Clean Growth Innovation Fund Rate Rider per Day	\$0.0131	\$0.000	\$0.0131
4	Subtotal of per Day <b>Delivery Margin Related Charges</b>	<b>\$0.9616</b>	<b>\$0.000</b>	<b>\$0.9616</b>
5				
6	Delivery Charge per GJ	\$4.165	\$0.000	\$4.165
7	Rider 3 BVA Rate Rider per GJ	\$0.059	\$0.000	\$0.059
8	Rider 5 RSAM per GJ	\$0.012	\$0.000	\$0.012
9	Subtotal of Per GJ <b>Delivery Margin Related Charges</b>	<b>\$4.236</b>	<b>\$0.000</b>	<b>\$4.236</b>
10				
11				
12	<u>Commodity Related Charges</u>			
13	Storage and Transport Charge per GJ	\$1.542	\$0.000	\$1.542
14	Rider 6 MCRA per GJ	(\$0.158 )	\$0.000	(\$0.158 )
16	Subtotal <b>Storage and Transport Related Charges per GJ</b>	<b>\$1.384</b>	<b>\$0.000</b>	<b>\$1.384</b>
17				
18	<b>Cost of Gas (Commodity Cost Recovery Charge) per GJ</b>	<b>\$4.503</b>	<b>\$3.088</b>	<b>\$7.591</b>

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SCHEDULE 2B

RATE SCHEDULE 2B: SMALL COMMERCIAL BIOMETHANE SERVICE		EXISTING RATES JANUARY 1, 2022	COMMODITY RELATED CHARGES CHANGES	TESTED JULY 1, 2022 RATES
Line No.	Particulars	Mainland and Vancouver Island	Mainland and Vancouver Island	Mainland and Vancouver Island
	(1)	(2)	(3)	(4)
1	<u>Delivery Margin Related Charges</u>			
2	<b>Basic Charge per Day</b>	<b>\$0.9485</b>	<b>\$0.0000</b>	<b>\$0.9485</b>
3	Rider 2 Clean Growth Innovation Fund Rate Rider per Day	\$0.0131	\$0.000	\$0.0131
4	Subtotal of per Day <b>Delivery Margin Related Charges</b>	<b>\$0.9616</b>	<b>\$0.000</b>	<b>\$0.9616</b>
5				
6	Delivery Charge per GJ	\$4.165	\$0.000	\$4.165
7	Rider 3 BVA Rate Rider per GJ	\$0.059	\$0.000	\$0.059
8	Rider 5 RSAM per GJ	\$0.012	\$0.000	\$0.012
9	Subtotal of Per GJ <b>Delivery Margin Related Charges</b>	<b>\$4.236</b>	<b>\$0.000</b>	<b>\$4.236</b>
10				
11				
12	<u>Commodity Related Charges</u>			
13	Storage and Transport Charge per GJ	\$1.542	\$0.000	\$1.542
14	Rider 6 MCRA per GJ	(\$0.158 )	\$0.000	(\$0.158 )
16	Subtotal <b>Storage and Transport Related Charges per GJ</b>	<b>\$1.384</b>	<b>\$0.000</b>	<b>\$1.384</b>
17				
18	<b>Cost of Gas (Commodity Cost Recovery Charge) per GJ</b>	<b>\$4.503</b>	<b>\$3.088</b>	<b>\$7.591</b>
19				
20	<b>Cost of Biomethane per GJ</b>	<b>\$13.808</b>	<b>\$0.000</b>	<b>\$13.808</b>
21	(Biomethane Energy Recovery Charge)			

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SCHEDULE 3

RATE SCHEDULE 3: LARGE COMMERCIAL SERVICE		EXISTING RATES JANUARY 1, 2022	COMMODITY RELATED CHARGES CHANGES	TESTED JULY 1, 2022 RATES
Line No.	Particulars (1)	Mainland and Vancouver Island (2)	Mainland and Vancouver Island (3)	Mainland and Vancouver Island (4)
1	<u>Delivery Margin Related Charges</u>			
2	<b>Basic Charge per Day</b>	<b>\$4.7895</b>	<b>\$0.0000</b>	<b>\$4.7895</b>
3	Rider 2 Clean Growth Innovation Fund Rate Rider per Day	\$0.0131	\$0.000	\$0.0131
4	Subtotal of per Day <b>Delivery Margin Related Charges</b>	<b>\$4.8026</b>	<b>\$0.000</b>	<b>\$4.8026</b>
5				
6	Delivery Charge per GJ	\$3.582	\$0.000	\$3.582
7	Rider 3 BVA Rate Rider per GJ	\$0.059	\$0.000	\$0.059
8	Rider 5 RSAM per GJ	\$0.012	\$0.000	\$0.012
9	Subtotal of Per GJ <b>Delivery Margin Related Charges</b>	<b>\$3.653</b>	<b>\$0.000</b>	<b>\$3.653</b>
10				
11				
12	<u>Commodity Related Charges</u>			
13	Storage and Transport Charge per GJ	\$1.312	\$0.000	\$1.312
14	Rider 6 MCRA per GJ	(\$0.135 )	\$0.000	(\$0.135 )
16	Subtotal <b>Storage and Transport Related Charges per GJ</b>	<b>\$1.177</b>	<b>\$0.000</b>	<b>\$1.177</b>
17				
18	<b>Cost of Gas (Commodity Cost Recovery Charge) per GJ</b>	<b>\$4.503</b>	<b>\$3.088</b>	<b>\$7.591</b>

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SCHEDULE 3B

RATE SCHEDULE 3B: LARGE COMMERCIAL BIOMETHANE SERVICE		EXISTING RATES JANUARY 1, 2022	COMMODITY RELATED CHARGES CHANGES	TESTED JULY 1, 2022 RATES
Line No.	Particulars	Mainland and Vancouver Island	Mainland and Vancouver Island	Mainland and Vancouver Island
	(1)	(2)	(3)	(4)
1	<u>Delivery Margin Related Charges</u>			
2	<b>Basic Charge per Day</b>	<b>\$4.7895</b>	<b>\$0.0000</b>	<b>\$4.7895</b>
3	Rider 2 Clean Growth Innovation Fund Rate Rider per Day	\$0.0131	\$0.000	\$0.0131
4	Subtotal of per Day <b>Delivery Margin Related Charges</b>	<b>\$4.8026</b>	<b>\$0.000</b>	<b>\$4.8026</b>
5				
6	Delivery Charge per GJ	\$3.582	\$0.000	\$3.582
7	Rider 3 BVA Rate Rider per GJ	\$0.059	\$0.000	\$0.059
8	Rider 5 RSAM per GJ	\$0.012	\$0.000	\$0.012
9	Subtotal of Per GJ <b>Delivery Margin Related Charges</b>	<b>\$3.653</b>	<b>\$0.000</b>	<b>\$3.653</b>
10				
11				
12	<u>Commodity Related Charges</u>			
13	Storage and Transport Charge per GJ	\$1.312	\$0.000	\$1.312
14	Rider 6 MCRA per GJ	(\$0.135 )	\$0.000	(\$0.135 )
15	Subtotal <b>Storage and Transport Related Charges per GJ</b>	<b>\$1.177</b>	<b>\$0.000</b>	<b>\$1.177</b>
16				
17	<b>Cost of Gas (Commodity Cost Recovery Charge) per GJ</b>	<b>\$4.503</b>	<b>\$3.088</b>	<b>\$7.591</b>
18				
19	<b>Cost of Biomethane per GJ</b>	<b>\$13.808</b>	<b>\$0.000</b>	<b>\$13.808</b>
20	(Biomethane Energy Recovery Charge)			



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SCHEDULE 4

RATE SCHEDULE 4: SEASONAL FIRM GAS SERVICE		EXISTING RATES JANUARY 1, 2022	COMMODITY RELATED CHARGES CHANGES	TESTED JULY 1, 2022 RATES
Line No.	Particulars	Mainland and Vancouver Island	Mainland and Vancouver Island	Mainland and Vancouver Island
	(1)	(2)	(3)	(4)
1	<u>Delivery Margin Related Charges</u>			
2	<b>Basic Charge per Day</b>	\$14.4230	\$0.0000	\$14.4230
3	<b>Rider 2 Clean Growth Innovation Fund Rate Rider per Day</b>	\$0.0131	\$0.000	\$0.0131
4	Subtotal of per Day <b>Delivery Margin Related Charges</b>	<b>\$14.4361</b>	<b>\$0.000</b>	<b>\$14.4361</b>
5				
6	<b>Delivery Charge per GJ</b>			
7	(a) Off-Peak Period	\$1.723	\$0.000	\$1.723
8	(b) Extension Period	\$2.368	\$0.000	\$2.368
9				
10	<b>Rider 3 BVA Rate Rider per GJ</b>	\$0.059	\$0.000	\$0.059
11				
12	<u>Commodity Related Charges</u>			
13	<b>Commodity Cost Recovery Charge per GJ</b>			
14	(a) Off-Peak Period	\$4.503	\$3.088	\$7.591
15	(b) Extension Period	\$4.503	\$3.088	\$7.591
16				
17	<b>Storage and Transport Charge per GJ</b>			
18	(a) Off-Peak Period	\$0.912	\$0.000	\$0.912
19	(b) Extension Period	\$0.912	\$0.000	\$0.912
20				
21	<b>Rider 6 MCRA per GJ</b>	(\$0.094 )	\$0.000	(\$0.094 )
22				
23	Subtotal Commodity Related Charges per GJ			
24	(a) Off-Peak Period	\$5.321	\$3.088	\$8.409
25	(b) Extension Period	\$5.321	\$3.088	\$8.409
26				
27				
28				
29	Unauthorized Gas Charge per gigajoule			
30	during peak period			
31				
32				
33	Total Variable Cost per gigajoule between			
34	(a) Off-Peak Period	\$7.103	\$3.088	\$10.191
35	(b) Extension Period	\$7.748	\$3.088	\$10.836

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SCHEDULE 5

RATE SCHEDULE 5 GENERAL FIRM SERVICE		EXISTING RATES JANUARY 1, 2022	COMMODITY RELATED CHARGES CHANGES	TESTED JULY 1, 2022 RATES
Line No.	Particulars	Mainland and Vancouver Island	Mainland and Vancouver Island	Mainland and Vancouver Island
	(1)	(2)	(3)	(4)
1	<u>Delivery Margin Related Charges</u>			
2	<b>Basic Charge per Month</b>	\$469.00	\$0.00	\$469.00
3	<b>Rider 2 Clean Growth Innovation Fund Rate Rider per Month</b>	\$0.40	\$0.00	\$0.40
4	Subtotal of per Month <b>Delivery Margin Related Charges</b>	<b>\$469.40</b>	<b>\$0.00</b>	<b>\$469.40</b>
5				
6	<b>Demand Charge per Month per GJ of Daily Demand</b>	\$27.911	\$0.000	\$27.911
7				
8	<b>Delivery Charge per GJ</b>	\$1.000	\$0.000	\$1.000
9				
10	<b>Rider 3 BVA Rate Rider per GJ</b>	\$0.059	\$0.000	\$0.059
11				
12				
13	<u>Commodity Related Charges</u>			
14	<b>Cost of Gas (Commodity Cost Recovery Charge) per GJ</b>	\$4.503	\$3.088	\$7.591
15	<b>Storage and Transport Charge per GJ</b>	\$0.912	\$0.000	\$0.912
16	<b>Rider 6 MCRA per GJ</b>	(\$0.094)	\$0.000	(\$0.094)
17	Subtotal Commodity Related Charges per GJ	<b>\$5.321</b>	<b>\$3.088</b>	<b>\$8.409</b>
18				
19				
20				
21				
22	Total Variable Cost per gigajoule	<b>\$6.380</b>	<b>\$3.088</b>	<b>\$9.468</b>

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SCHEDULE 5B

RATE SCHEDULE 5B: GENERAL FIRM BIOMETHANE SERVICE		EXISTING RATES JANUARY 1, 2022	COMMODITY RELATED CHARGES CHANGES	TESTED JULY 1, 2022 RATES
Line No.	Particulars	Mainland and Vancouver Island	Mainland and Vancouver Island	Mainland and Vancouver Island
	(1)	(2)	(3)	(4)
1	<u>Delivery Margin Related Charges</u>			
2	<b>Basic Charge per Month</b>	\$469.00	\$0.00	\$469.00
3	<b>Rider 2 Clean Growth Innovation Fund Rate Rider per Month</b>	\$0.40	\$0.00	\$0.40
4	Subtotal of per Month <b>Delivery Margin Related Charges</b>	<b>\$469.40</b>	<b>\$0.00</b>	<b>\$469.40</b>
5				
6	<b>Demand Charge per GJ</b>	\$27.911	\$0.000	\$27.911
7				
8	<b>Delivery Charge per GJ</b>	\$1.000	\$0.000	\$1.000
9				
10	<b>Rider 3 BVA Rate Rider per GJ</b>	\$0.059	\$0.000	\$0.059
11				
12				
13	<u>Commodity Related Charges</u>			
14	Storage and Transport Charge per GJ	\$0.912	\$0.000	\$0.912
15	Rider 6 MCRA per GJ	(\$0.094)	\$0.000	(\$0.094)
16	Subtotal <b>Storage and Transport Related Charges per GJ</b>	<b>\$0.818</b>	<b>\$0.000</b>	<b>\$0.818</b>
17				
18	<b>Cost of Gas (Commodity Cost Recovery Charge) per GJ</b>	<b>\$4.503</b>	<b>\$3.088</b>	<b>\$7.591</b>
19				
20	<b>Cost of Biomethane per GJ</b>	<b>\$13.808</b>	<b>\$0.000</b>	<b>\$13.808</b>
21	(Biomethane Energy Recovery Charge)			

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SCHEDULE 6

RATE SCHEDULE 6: NATURAL GAS VEHICLE SERVICE		EXISTING RATES JANUARY 1, 2022	COMMODITY RELATED CHARGES CHANGES	TESTED JULY 1, 2022 RATES
Line No.	Particulars	Mainland and Vancouver Island	Mainland and Vancouver Island	Mainland and Vancouver Island
	(1)	(2)	(3)	(4)
1	<u>Delivery Margin Related Charges</u>			
2	<b>Basic Charge per Day</b>	\$2.0041	\$0.0000	\$2.0041
3	<b>Rider 2 Clean Growth Innovation Fund Rate Rider per Day</b>	\$0.0131	\$0.000	\$0.0131
4	Subtotal of per Day <b>Delivery Margin Related Charges</b>	<b>\$2.0172</b>	<b>\$0.000</b>	<b>\$2.0172</b>
5				
6	<b>Delivery Charge per GJ</b>	\$3.446	\$0.000	\$3.446
7				
8	<b>Rider 3 BVA Rate Rider per GJ</b>	\$0.059	\$0.000	\$0.059
9				
10				
11	<u>Commodity Related Charges</u>			
12	<b>Cost of Gas (Commodity Cost Recovery Charge) per GJ</b>	\$4.503	\$3.088	\$7.591
13	<b>Storage and Transport Charge per GJ</b>	\$0.470	\$0.000	\$0.470
14	<b>Rider 6 MCRA per GJ</b>	(\$0.048 )	\$0.000	(\$0.048 )
15	Subtotal Commodity Related Charges per GJ	<b>\$4.925</b>	<b>\$3.088</b>	<b>\$8.013</b>
16				
17				
18	Total Variable Cost per gigajoule	<u>\$8.430</u>	<u>\$3.088</u>	<u>\$11.518</u>

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SCHEDULE 6P - Surrey

RATE SCHEDULE 6P: PUBLIC SERVICE - NATURAL GAS VEHICLE REFUELING SERVICE				
Line No.	Particulars	EXISTING RATES JANUARY 1, 2022	COMMODITY RELATED CHARGES CHANGES	TESTED JULY 1, 2022 RATES
	(1)	(2)	(3)	(4)
1	<b>Surrey Fueling Station</b>			
2				
3	<b><u>Delivery Margin Related Charges</u></b>			
4	Delivery Charge per GJ	\$3.446	\$0.000	\$3.446
5	Rider 3 BVA Rate Rider per GJ	\$0.059	\$0.000	\$0.059
6	Subtotal of per Gigajoule <b>Delivery Margin Related Charges</b>	<b>\$3.505</b>	<b>\$0.000</b>	<b>\$3.505</b>
7				
8	<b><u>Commodity Related Charges</u></b>			
9	Cost of Gas (Commodity Cost Recovery Charge) per GJ	\$4.503	\$3.088	\$7.591
10	Storage and Transport Charge per GJ	\$0.470	\$0.000	\$0.470
11	Rider 6 MCRA per GJ	(\$0.048 )	\$0.000	(\$0.048 )
12	Subtotal Commodity Related Charges per GJ	<b>\$4.925</b>	<b>\$3.088</b>	<b>\$8.013</b>
13				
14				
15	<b><u>Station Service Related Charges</u></b>			
16	Compression Charge per gigajoule	\$8.441	\$0.000	\$8.441
17	Subtotal of per Gigajoule <b>Station Service Related Charges</b>	<b>\$8.441</b>	<b>\$0.000</b>	<b>\$8.441</b>
18				
19				
20	Total per Gigajoule Rate	<b>\$16.871</b>	<b>\$3.088</b>	<b>\$19.959</b>

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SCHEDULE 6P - 360S

RATE SCHEDULE 6P: PUBLIC SERVICE - NATURAL GAS VEHICLE REFUELING SERVICE				
Line No.	Particulars	EXISTING RATES JANUARY 1, 2022	COMMODITY RELATED CHARGES CHANGES	TESTED JULY 1, 2022 RATES
	(1)	(2)	(3)	(4)
1	<sup>1</sup> E360S Fueling Station			
2				
3	<b><u>Delivery Margin Related Charges</u></b>			
4	Delivery Charge per GJ	\$2.301	\$0.000	\$2.301
5	Rider 3 BVA Rate Rider per GJ	\$0.059	\$0.000	\$0.059
6	Subtotal of per Gigajoule <b>Delivery Margin Related Charges</b>	<b>\$2.36</b>	<b>\$0.000</b>	<b>\$2.360</b>
7				
8	<b><u>Commodity Related Charges</u></b>			
9	Cost of Gas (Commodity Cost Recovery Charge) per GJ	\$4.503	\$3.088	\$7.591
10	Storage and Transport Charge per GJ	\$0.912	\$0.000	\$0.912
11	Rider 6 MCRA per GJ	(\$0.094 )	\$0.000	(\$0.094 )
12	Subtotal Commodity Related Charges per GJ	<b>\$5.321</b>	<b>\$3.088</b>	<b>\$8.409</b>
13				
14				
15	<b><u>Station Service Related Charges</u></b>			
16	Capital Rate per gigajoule	\$3.090	\$0.000	\$3.090
17	O&M Rate per gigajoule	\$2.559	\$0.000	\$2.559
18	OH&M per gigajoule	\$0.520	\$0.000	\$0.520
19	Short Term Charge per gigajoule	\$1.000	\$0.000	\$1.000
20	Spot Charge per gigajoule	\$1.000	\$0.000	\$1.000
21	Host Fee per gigajoule	\$2.500	\$0.000	\$2.500
22	Subtotal of per Gigajoule <b>Station Service Related Charges</b>	<b>\$10.669</b>	<b>\$0.000</b>	<b>\$10.669</b>
23				
24				
25	Total per Gigajoule Rate	<b>\$18.350</b>	<b>\$3.088</b>	<b>\$21.438</b>

<sup>1</sup> Pursuant to BCUC Order G-200-20, rates were originally approved on an interim basis effective July 31, 2020.

FORTISBC ENERGY INC.  
CALCULATION OF CUSTOMERS' RATES AND TARIFF CONTINUITY  
TESTED JULY 1, 2022 RATES  
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TAB 5  
PAGE 10.4  
SCHEDULE 6P - Annacis

RATE SCHEDULE 6P: PUBLIC SERVICE - NATURAL GAS VEHICLE REFUELING SERVICE				
Line No.	Particulars	EXISTING RATES NOVEMBER 30, 2021	COMMODITY RELATED CHARGES CHANGES	TESTED JULY 1, 2022 RATES
	(1)	(2)	(3)	(4)
1	<sup>1</sup> Annacis Fueling Station			
2				
3	<b><u>Delivery Margin Related Charges</u></b>			
4	Delivery Charge per GJ	\$2.369	\$0.000	\$2.369
5	Rider 3 BVA Rate Rider per GJ	\$0.059	\$0.000	\$0.059
6	Subtotal of per Gigajoule <b>Delivery Margin Related Charges</b>	<b>\$2.428</b>	<b>\$0.000</b>	<b>\$2.428</b>
7				
8	<b><u>Commodity Related Charges</u></b>			
9	Cost of Gas (Commodity Cost Recovery Charge) per GJ	\$4.503	\$3.088	\$7.591
10	Storage and Transport Charge per GJ	\$0.912	\$0.000	\$0.912
11	Rider 6 MCRA per GJ	(\$0.094 )	\$0.000	(\$0.094 )
12	Subtotal Commodity Related Charges per GJ	<b>\$5.321</b>	<b>\$3.088</b>	<b>\$8.409</b>
13				
14				
15	<b><u>Station Service Related Charges</u></b>			
16	Capital Rate per gigajoule	\$4.382	\$0.000	\$4.382
17	O&M Rate per gigajoule	\$2.686	\$0.000	\$2.686
18	OH&M per gigajoule	\$0.520	\$0.000	\$0.520
19	Short Term Charge per gigajoule	\$1.000	\$0.000	\$1.000
20	Spot Charge per gigajoule	\$1.000	\$0.000	\$1.000
21	Host Fee per gigajoule	\$2.500	\$0.000	\$2.500
22	Subtotal of per Gigajoule <b>Station Service Related Charges</b>	<b>\$12.088</b>	<b>\$0.000</b>	<b>\$12.088</b>
23				
24				
25	Total per Gigajoule Rate	<b>\$19.837</b>	<b>\$3.088</b>	<b>\$22.925</b>

<sup>1</sup> Pursuant to BCUC Order G-313-21, rates were approved on an interim basis effective November 30, 2021.

FORTISBC ENERGY INC.  
CALCULATION OF CUSTOMERS' RATES AND TARIFF CONTINUITY  
TESTED JULY 1, 2022 RATES  
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SCHEDULE 6P - GFL

RATE SCHEDULE 6P: PUBLIC SERVICE - NATURAL GAS VEHICLE REFUELING SERVICE				
Line No.	Particulars	EXISTING RATES NOVEMBER 30, 2021	COMMODITY RELATED CHARGES CHANGES	TESTED JULY 1, 2022 RATES
	(1)	(2)	(3)	(4)
1	<b>GFL Abbotsford Fueling Station</b>			
2				
3	<b><u>Delivery Margin Related Charges</u></b>			
4	Delivery Charge per GJ	\$2.867	\$0.000	\$2.867
5	Rider 3 BVA Rate Rider per GJ	\$0.059	\$0.000	\$0.059
6	Subtotal of per Gigajoule <b>Delivery Margin Related Charges</b>	<b>\$2.926</b>	<b>\$0.000</b>	<b>\$2.926</b>
7				
8	<b><u>Commodity Related Charges</u></b>			
9	Cost of Gas (Commodity Cost Recovery Charge) per GJ	\$4.503	\$3.088	\$7.591
10	Storage and Transport Charge per GJ	\$0.912	\$0.000	\$0.912
11	Rider 6 MCRA per GJ	(\$0.094 )	\$0.000	(\$0.094 )
12	Subtotal Commodity Related Charges per GJ	<b>\$5.321</b>	<b>\$3.088</b>	<b>\$8.409</b>
13				
14				
15	<b><u>Station Service Related Charges</u></b>			
16	Capital Rate per gigajoule	\$6.656	\$0.000	\$6.656
17	O&M Rate per gigajoule	\$2.014	\$0.000	\$2.014
18	OH&M per gigajoule	\$0.520	\$0.000	\$0.520
19	Short Term Charge per gigajoule	\$1.000	\$0.000	\$1.000
20	Spot Charge per gigajoule	\$1.000	\$0.000	\$1.000
21	Host Fee per gigajoule	\$2.500	\$0.000	\$2.500
22	Subtotal of per Gigajoule <b>Station Service Related Charges</b>	<b>\$13.690</b>	<b>\$0.000</b>	<b>\$13.690</b>
23				
24				
25	Total per Gigajoule Rate	<b>\$21.937</b>	<b>\$3.088</b>	<b>\$25.025</b>

<sup>1</sup> Pursuant to BCUC Order G-330-21, rates were approved on an interim basis effective November 30, 2021.



FORTISBC ENERGY INC.  
CALCULATION OF CUSTOMERS' RATES AND TARIFF CONTINUITY  
TESTED JULY 1, 2022 RATES  
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SCHEDULE 7

RATE SCHEDULE 7: GENERAL INTERRUPTIBLE SERVICE		EXISTING RATES JANUARY 1, 2022	COMMODITY RELATED CHARGES CHANGES	TESTED JULY 1, 2022 RATES
Line No.	Particulars	Mainland and Vancouver Island	Mainland and Vancouver Island	Mainland and Vancouver Island
	(1)	(2)	(3)	(4)
1	<u>Delivery Margin Related Charges</u>			
2	<b>Basic Charge per Month</b>	\$880.00	\$0.00	\$880.00
3	<b>Rider 2 Clean Growth Innovation Fund Rate Rider per Month</b>	\$0.40	\$0.00	\$0.40
4	Subtotal of per Month <b>Delivery Margin Related Charges</b>	<b>\$880.40</b>	<b>\$0.00</b>	<b>\$880.40</b>
5				
6	<b>Delivery Charge per GJ</b>	\$1.616	\$0.000	\$1.616
7				
8	<b>Rider 3 BVA Rate Rider per GJ</b>	\$0.059	\$0.000	\$0.059
9				
10	<u>Commodity Related Charges</u>			
11	<b>Cost of Gas (Commodity Cost Recovery Charge) per GJ</b>	\$4.503	\$3.088	\$7.591
12	<b>Storage and Transport Charge per GJ</b>	\$0.912	\$0.000	\$0.912
13	<b>Rider 6 MCRA per GJ</b>	(\$0.094)	\$0.000	(\$0.094)
14	Subtotal Commodity Related Charges per GJ	<b>\$5.321</b>	<b>\$3.088</b>	<b>\$8.409</b>
15				
16				
17	Total Variable Cost per gigajoule	<u>\$6.996</u>	<u>\$3.088</u>	<u>\$10.084</u>

FORTISBC ENERGY INC.  
CALCULATION OF CUSTOMERS' RATES AND TARIFF CONTINUITY  
TESTED JULY 1, 2022 RATES  
BCUC ORDERS G-367-21 G-354-21

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SCHEDULE 46.1

RATE SCHEDULE 46: LNG SERVICE		EXISTING RATES JANUARY 1, 2022	COMMODITY RELATED CHARGES CHANGES	TESTED JULY 1, 2022 RATES
Line No.	Particulars	Mainland and Vancouver Island	Mainland and Vancouver Island	Mainland and Vancouver Island
	(1)	(2)	(3)	(4)
1	<u>Dispensing Service Charges per GJ</u>			
2	<b>LNG Facility Charge per GJ</b>	\$4.21	\$0.00	\$4.21
3	<b>Electricity Surcharge per GJ</b>	\$1.04	\$0.00	\$1.04
4	<b>LNG Spot Charge per GJ</b>	\$5.50	\$0.00	\$5.50
5				
6				
7	<u>Commodity Related Charges</u>			
8	Storage and Transport Charge per GJ	\$0.912	\$0.000	\$0.912
9	Rider 6 MCRA per GJ	(\$0.094)	\$0.000	(\$0.094)
10	Subtotal Storage and Transport Related Charges per GJ	<b>\$0.818</b>	<b>\$0.000</b>	<b>\$0.818</b>
11				
12	<b>Cost of Gas (Commodity Cost Recovery Charge) per GJ</b>	<b>\$4.503</b>	<b>\$3.088</b>	<b>\$7.591</b>
13				
14	<b>Cost of Biomethane per GJ</b>	<b>\$13.808</b>	<b>\$0.000</b>	<b>\$13.808</b>
15	(Biomethane Energy Recovery Charge)			
16				
17				
18	Total Variable Cost per gigajoule (excluding LNG Spot Charge per GJ)	<u>\$10.571</u>	<u>\$3.088</u>	<u>\$13.659</u>

FORTISBC ENERGY INC.  
COMMODITY RELATED CHARGES CHANGES  
BCUC ORDERS G-XX-22  
**RATE SCHEDULE 1 - RESIDENTIAL SERVICE**

Line No.	Particular	EXISTING RATES JANUARY 1, 2022					TESTED JULY 1, 2022 RATES					Annual Increase/Decrease		
		Quantity		Rate		Annual \$	Quantity		Rate		Annual \$	Rate	Annual \$	% of Previous Total Annual Bill
1	<b>MAINLAND AND VANCOUVER ISLAND SERVICE AREA</b>													
2	<u>Delivery Margin Related Charges</u>													
3	Basic Charge per Day	365.25	days x	\$0.4085	=	\$149.20	365.25	days x	\$0.4085	=	\$149.20	\$0.0000	\$0.0000	0.00%
4	Rider 2 Clean Growth Innovation Fund Rate Rider per Day	365.25	days x	\$0.0131	=	4.78	365.25	days x	\$0.0131	=	4.78	0.00	0.00	0.00%
5	Subtotal of per Day Delivery Margin Related Charges					<b>\$153.98</b>					<b>\$153.98</b>		<b>\$0.00</b>	<b>0.00%</b>
6														
7	Delivery Charge per GJ	90.0	GJ x	\$5.455	=	490.9500	90.0	GJ x	\$5.455	=	490.9500	\$0.000	\$0.0000	0.00%
8	Rider 3 BVA Rate Rider per GJ	90.0	GJ x	\$0.059	=	5.3100	90.0	GJ x	\$0.059	=	5.3100	\$0.000	0.0000	0.00%
9	Rider 5 RSAM per GJ	90.0	GJ x	\$0.012	=	1.0800	90.0	GJ x	\$0.012	=	1.0800	\$0.000	0.0000	0.00%
10	Subtotal of Per GJ Delivery Margin Related Charges					<b>\$497.34</b>					<b>\$497.34</b>		<b>\$0.00</b>	<b>0.00%</b>
11														
12	<u>Commodity Related Charges</u>													
13	Storage and Transport Charge per GJ	90.0	GJ x	\$1.505	=	\$135.4500	90.0	GJ x	\$1.505	=	\$135.4500	\$0.000	\$0.0000	0.00%
14	Rider 6 MCRA per GJ	90.0	GJ x	(\$0.154 )	=	(13.8600)	90.0	GJ x	(\$0.154 )	=	(13.8600)	\$0.000	0.0000	0.00%
15	Subtotal Storage and Transport Related Charges per GJ					\$121.59					\$121.59		\$0.00	0.00%
16														
17	Cost of Gas (Commodity Cost Recovery Charge) per GJ	90.0	GJ x	\$4.503	=	\$405.27	90.0	GJ x	\$7.591	=	\$683.19	\$3.088	\$277.9200	23.59%
18	Subtotal Commodity Related Charges per GJ					<b>\$526.86</b>					<b>\$804.78</b>		<b>\$277.92</b>	<b>23.59%</b>
19														
20	Total (with effective \$/GJ rate)	90.0		\$13.091		<b>\$1,178.18</b>	90.0		\$16.179		<b>\$1,456.10</b>	\$3.088	<b>\$277.92</b>	<b>23.59%</b>

Notes: Tariff rate schedule per GJ charges are set at 3 decimals. Individual tariff components are calculated and shown to 4 decimals; subtotal amounts, equivalent to the line items on customer bills, are rounded and shown to 2 decimals, consistent with actual invoice calculations.  
Slight differences in totals due to rounding

FORTISBC ENERGY INC.  
COMMODITY RELATED CHARGES CHANGES  
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RATE SCHEDULE 1B - RESIDENTIAL BIOMETHANE SERVICE

Line No.	Particular	EXISTING RATES JANUARY 1, 2022				TESTED JULY 1, 2022 RATES				Annual Increase/Decrease		
		Quantity		Rate	Annual \$	Quantity		Rate	Annual \$	Rate	Annual \$	% of Previous Total Annual Bill
1	MAINLAND AND VANCOUVER ISLAND SERVICE AREA											
2	Delivery Margin Related Charges											
3	Basic Charge per Day	365.25	days x	\$0.4085	= \$149.20	365.25	days x	\$0.4085	= \$149.20	\$0.0000	\$0.00	0.00%
4	Rider 2 Clean Growth Innovation Fund Rate Rider per Day	365.25	days x	\$0.0131	= 4.78	365.25	days x	\$0.0131	= 4.78	\$0.000	0.00	0.00%
5	Subtotal of per Day Delivery Margin Related Charges				\$153.98				\$153.98		\$0.00	0.00%
6												
7	Delivery Charge per GJ	90.0	GJ x	\$5.455	= 490.9500	90.0	GJ x	\$5.455	= 490.9500	\$0.000	0.0000	0.00%
8	Rider 3 BVA Rate Rider per GJ	90.0	GJ x	\$0.059	= 5.3100	90.0	GJ x	\$0.059	= 5.3100	\$0.000	0.0000	0.00%
9	Rider 5 RSAM per GJ	90.0	GJ x	\$0.012	= 1.0800	90.0	GJ x	\$0.012	= 1.0800	\$0.000	0.0000	0.00%
10	Subtotal of Per GJ Delivery Margin Related Charges				\$497.34				\$497.34		\$0.00	0.00%
11	Commodity Related Charges											
12	Storage and Transport Charge per GJ	90.0	GJ x	\$1.505	= \$135.4500	90.0	GJ x	\$1.505	= \$135.4500	\$0.000	\$0.0000	0.00%
13	Rider 6 MCRA per GJ	90.0	GJ x	(\$0.154 )	= (13.8600)	90.0	GJ x	(\$0.154 )	= (13.8600)	\$0.000	0.0000	0.00%
14	Subtotal Storage and Transport Related Charges per GJ				\$121.59				\$121.59		\$0.00	0.00%
15	Cost of Gas (Commodity Cost Recovery Charge) per GJ	90.0	GJ x 90% x	\$4.503	= 364.74	90.0	GJ x 90% x	\$7.591	= 614.87	\$3.088	250.13	19.82%
16	Cost of Biomethane	90.0	GJ x 10% x	\$13.808	= 124.27	90.0	GJ x 10% x	\$13.808	= 124.27	\$0.000	0.00	0.00%
17	Subtotal Commodity Related Charges				\$610.60				\$860.73		\$250.13	19.82%
18												
19	Total (with effective \$/GJ rate)	90.0		\$14.021	\$1,261.92	90.0		\$16.801	\$1,512.05	\$2.779	\$250.13	19.82%

Notes: Commodity Cost Recovery Related Charge is based on 90% of the Cost of Gas (Commodity Cost Related Charge) per GJ and 10% of the Cost of Biomethane per GJ.

Notes: Tariff rate schedule per GJ charges are set at 3 decimals. Individual tariff components are calculated and shown to 4 decimals; subtotal amounts, equivalent to the line items on customer bills, are rounded and shown to 2 decimals, consistent with actual invoice calculations.

Slight differences in totals due to rounding

FORTISBC ENERGY INC.  
COMMODITY RELATED CHARGES CHANGES  
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**RATE SCHEDULE 2 - SMALL COMMERCIAL SERVICE**

Line No.	Particular	EXISTING RATES JANUARY 1, 2022					TESTED JULY 1, 2022 RATES					Annual Increase/Decrease		
		Quantity		Rate		Annual \$	Quantity		Rate		Annual \$	Rate	Annual \$	% of Previous Total Annual Bill
1	<b>MAINLAND AND VANCOUVER ISLAND SERVICE AREA</b>													
2	<u>Delivery Margin Related Charges</u>													
3	Basic Charge per Day	365.25	days x	\$0.9485	=	\$346.44	365.25	days x	\$0.9485	=	\$346.44	\$0.0000	\$0.00	0.00%
4	Rider 2 Clean Growth Innovation Fund Rate Rider per Day	365.25	days x	\$0.0131	=	4.78	365.25	days x	\$0.0131	=	4.78	\$0.000	0.00	0.00%
5	Subtotal of per Day Delivery Margin Related Charges					<b>\$351.22</b>					<b>\$351.22</b>		<b>\$0.00</b>	<b>0.00%</b>
6														
7	Delivery Charge per GJ	340.0	GJ x	\$4.165	=	1,416.1000	340.0	GJ x	\$4.165	=	1,416.1000	\$0.000	0.0000	0.00%
8	Rider 3 BVA Rate Rider per GJ	340.0	GJ x	\$0.059	=	20.0600	340.0	GJ x	\$0.059	=	20.0600	\$0.000	0.0000	0.00%
9	Rider 5 RSAM per GJ	340.0	GJ x	\$0.012	=	4.0800	340.0	GJ x	\$0.012	=	4.0800	\$0.000	0.0000	0.00%
10	Subtotal of Per GJ Delivery Margin Related Charges					<b>\$1,440.24</b>					<b>\$1,440.24</b>		<b>\$0.00</b>	<b>0.00%</b>
11														
12	<u>Commodity Related Charges</u>													
13	Storage and Transport Charge per GJ	340.0	GJ x	\$1.542	=	\$524.2800	340.0	GJ x	\$1.542	=	\$524.2800	\$0.000	\$0.0000	0.00%
14	Rider 6 MCRA per GJ	340.0	GJ x	(\$0.158 )	=	(53.7200)	340.0	GJ x	(\$0.158 )	=	(53.7200)	\$0.000	0.0000	0.00%
15	Subtotal Storage and Transport Related Charges per GJ					\$470.56					\$470.56		\$0.00	0.00%
16														
17	Cost of Gas (Commodity Cost Recovery Charge) per GJ	340.0	GJ x	\$4.503	=	\$1,531.02	340.0	GJ x	\$7.591	=	\$2,580.94	\$3.088	\$1,049.92	27.68%
18	Subtotal Commodity Related Charges per GJ					<b>\$2,001.58</b>					<b>\$3,051.50</b>		<b>\$1,049.92</b>	<b>27.68%</b>
19														
20	Total (with effective \$/GJ rate)	340.0		\$11.156		<b>\$3,793.04</b>	340.0		\$14.244		<b>\$4,842.96</b>	\$3.088	<b>\$1,049.92</b>	<b>27.68%</b>

Notes: Tariff rate schedule per GJ charges are set at 3 decimals. Individual tariff components are calculated and shown to 4 decimals; subtotal amounts, equivalent to the line items on customer bills, are rounded and shown to 2 decimals, consistent with actual invoice calculations.  
Slight differences in totals due to rounding

FORTISBC ENERGY INC.  
COMMODITY RELATED CHARGES CHANGES  
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RATE SCHEDULE 2B-SMALL COMMERCIAL BIOMETHANE SERVICE

Line No.	Particular	EXISTING RATES JANUARY 1, 2022			TESTED JULY 1, 2022 RATES			Annual Increase/Decrease		
		Quantity	Rate	Annual \$	Quantity	Rate	Annual \$	Rate	Annual \$	% of Previous Total Annual Bill
1	MAINLAND AND VANCOUVER ISLAND SERVICE AREA									
2	Delivery Margin Related Charges									
3	Basic Charge per Day	365.25	days x	\$0.9485 =	\$346.44	365.25	days x	\$0.9485 =	\$346.44	\$0.0000 \$0.00 0.00%
4	Rider 2 Clean Growth Innovation Fund Rate Rider per Day	365.25	days x	\$0.0131 =	4.78	365.25	days x	\$0.0131 =	4.78	\$0.000 0.0000 0.00%
5	Subtotal of per Day Delivery Margin Related Charges				\$351.22				\$0.00	0.00%
6										
7	Delivery Charge per GJ	340.0	GJ x	\$4.165 =	1,416.1000	340.0	GJ x	\$4.165 =	1,416.1000	\$0.000 0.0000 0.00%
8	Rider 3 BVA Rate Rider per GJ	340.0	GJ x	\$0.059 =	20.0600	340.0	GJ x	\$0.059 =	20.0600	\$0.000 0.0000 0.00%
9	Rider 5 RSAM per GJ	340.0	GJ x	\$0.012 =	4.0800	340.0	GJ x	\$0.012 =	4.0800	\$0.000 0.0000 0.00%
10	Subtotal of Per GJ Delivery Margin Related Charges				\$1,440.24				\$0.00	0.00%
11										
12	Commodity Related Charges									
13	Storage and Transport Charge per GJ	340.0	GJ x	\$1.542 =	\$524.2800	340.0	GJ x	\$1.542 =	\$524.2800	\$0.000 \$0.0000 0.00%
14	Rider 6 MCRA per GJ	340.0	GJ x	(\$0.158 ) =	(53.7200)	340.0	GJ x	(\$0.158 ) =	(53.7200)	\$0.000 0.0000 0.00%
16	Subtotal Storage and Transport Related Charges per GJ				\$470.56				\$470.56	\$0.00 0.00%
17	Cost of Gas (Commodity Cost Recovery Charge) per GJ	340.0	GJ x 90% x	\$4.503 =	\$1,377.9200	340.0	GJ x 90% x	\$7.591 =	\$2,322.8500	\$3.088 944.93 22.99%
18	Cost of Biomethane	340.0	GJ x 10% x	\$13.808 =	469.4700	340.0	GJ x 10% x	\$13.808 =	469.4700	\$0.000 0.00 0.00%
19	Subtotal Commodity Related Charges per GJ				\$2,317.95				\$3,262.88	\$944.93 22.99%
20	Total (with effective \$/GJ rate)	340.0		\$12.087	\$4,109.41	340.0		\$14.866	\$5,054.34	\$2.779 \$944.93 22.99%

Notes: Commodity Cost Recovery Related Charge is based on 90% of the Cost of Gas (Commodity Cost Related Charge) per GJ and 10% of the Cost of Biomethane per GJ.

Notes: Tariff rate schedule per GJ charges are set at 3 decimals. Individual tariff components are calculated and shown to 4 decimals; subtotal amounts, equivalent to the line items on customer bills, are rounded and shown to 2 decimals, consistent with actual invoice calculations.

Slight differences in totals due to rounding

FORTISBC ENERGY INC.  
COMMODITY RELATED CHARGES CHANGES  
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**RATE SCHEDULE 3 - LARGE COMMERCIAL SERVICE**

Line No.	Particular	EXISTING RATES JANUARY 1, 2022					TESTED JULY 1, 2022 RATES					Annual Increase/Decrease		
		Quantity		Rate		Annual \$	Quantity		Rate		Annual \$	Rate	Annual \$	% of Previous Total Annual Bill
1	<b>MAINLAND AND VANCOUVER ISLAND SERVICE AREA</b>													
2	<u>Delivery Margin Related Charges</u>													
3	Basic Charge per Day	365.25	days x	\$4.7895	=	\$1,749.36	365.25	days x	\$4.7895	=	\$1,749.36	\$0.0000	\$0.00	0.00%
4	Rider 2 Clean Growth Innovation Fund Rate Rider per Day	365.25	days x	\$0.0131	=	4.78	365.25	days x	\$0.0131	=	4.78	\$0.000	0.00	0.00%
5	Subtotal of per Day Delivery Margin Related Charges					<b>\$1,754.14</b>					<b>\$1,754.14</b>		<b>\$0.00</b>	<b>0.00%</b>
6														
7	Delivery Charge per GJ	3,770.0	GJ x	\$3.582	=	13,504.1400	3,770.0	GJ x	\$3.582	=	13,504.1400	\$0.000	0.0000	0.00%
8	Rider 3 BVA Rate Rider per GJ	3,770.0	GJ x	\$0.059	=	222.4300	3,770.0	GJ x	\$0.059	=	222.4300	\$0.000	0.0000	0.00%
9	Rider 5 RSAM per GJ	3,770.0	GJ x	\$0.012	=	45.2400	3,770.0	GJ x	\$0.012	=	45.2400	\$0.000	0.0000	0.00%
10	Subtotal of Per GJ Delivery Margin Related Charges					<b>\$13,771.81</b>					<b>\$13,771.81</b>		<b>\$0.00</b>	<b>0.00%</b>
11														
12	<u>Commodity Related Charges</u>													
13	Storage and Transport Charge per GJ	3,770.0	GJ x	\$1.312	=	\$4,946.2400	3,770.0	GJ x	\$1.312	=	\$4,946.2400	\$0.000	\$0.0000	0.00%
14	Rider 6 MCRA per GJ	3,770.0	GJ x	(\$0.135 )	=	(508.9500)	3,770.0	GJ x	(\$0.135 )	=	(508.9500)	\$0.000	0.0000	0.00%
15	Subtotal Storage and Transport Related Charges per GJ					\$4,437.29					\$4,437.29		\$0.00	0.00%
16														
17	Cost of Gas (Commodity Cost Recovery Charge) per GJ	3,770.0	GJ x	\$4.503	=	\$16,976.31	3,770.0	GJ x	\$7.591	=	\$28,618.07	\$3.088	\$11,641.76	31.52%
18	Subtotal Commodity Related Charges per GJ					<b>\$21,413.60</b>					<b>\$33,055.36</b>		<b>\$11,641.76</b>	<b>31.52%</b>
19														
20	Total (with effective \$/GJ rate)	3,770.0		\$9.798		<b>\$36,939.55</b>	3,770.0		\$12.886		<b>\$48,581.31</b>	\$3.088	<b>\$11,641.76</b>	<b>31.52%</b>

Notes: Tariff rate schedule per GJ charges are set at 3 decimals. Individual tariff components are calculated and shown to 4 decimals; subtotal amounts, equivalent to the line items on customer bills, are rounded and shown to 2 decimals, consistent with actual invoice calculations.  
Slight differences in totals due to rounding

FORTISBC ENERGY INC.  
COMMODITY RELATED CHARGES CHANGES  
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**RATE SCHEDULE 3B - LARGE COMMERCIAL BIOMETHANE SERVICE**

Line No.	Particular	EXISTING RATES JANUARY 1, 2022					TESTED JULY 1, 2022 RATES					Annual Increase/Decrease		
		Quantity		Rate		Annual \$	Quantity		Rate		Annual \$	Rate	Annual \$	% of Previous Total Annual Bill
1	<b>MAINLAND AND VANCOUVER ISLAND SERVICE AREA</b>													
2	<u>Delivery Margin Related Charges</u>													
3	Basic Charge per Day	365.25	days x	\$4.7895	=	\$1,749.36	365.25	days x	\$4.7895	=	\$1,749.36	\$0.0000	\$0.00	0.00%
4	Rider 2 Clean Growth Innovation Fund Rate Rider per Day	365.25	days x	\$0.0131	=	4.78	365.25	days x	\$0.0131	=	4.78	\$0.000	0.00	0.00%
5	Subtotal of per Day Delivery Margin Related Charges					<b>\$1,754.14</b>					<b>\$1,754.14</b>		<b>\$0.00</b>	<b>0.00%</b>
6														
7	Delivery Charge per GJ	3,770.0	GJ x	\$3.582	=	13,504.1400	3,770.0	GJ x	\$3.582	=	13,504.1400	\$0.000	0.0000	0.00%
8	Rider 3 BVA Rate Rider per GJ	3,770.0	GJ x	\$0.059	=	222.4300	3,770.0	GJ x	\$0.059	=	222.4300	\$0.000	0.0000	0.00%
9	Rider 5 RSAM per GJ	3,770.0	GJ x	\$0.012	=	45.2400	3,770.0	GJ x	\$0.012	=	45.2400	\$0.000	0.0000	0.00%
10	Subtotal of Per GJ Delivery Margin Related Charges					<b>\$13,771.81</b>					<b>\$13,771.81</b>		<b>\$0.00</b>	<b>0.00%</b>
11														
12	<u>Commodity Related Charges</u>													
13	Storage and Transport Charge per GJ	3,770.0	GJ x	\$1.312	=	\$4,946.2400	3,770.0	GJ x	\$1.312	=	\$4,946.2400	\$0.000	\$0.0000	0.00%
14	Rider 6 MCRA per GJ	3,770.0	GJ x	(\$0.135 )	=	(508.9500)	3,770.0	GJ x	(\$0.135 )	=	(508.9500)	\$0.000	0.0000	0.00%
16	Subtotal Storage and Transport Related Charges per GJ					\$4,437.29					\$4,437.29		\$0.00	0.00%
17	Cost of Gas (Commodity Cost Recovery Charge) per GJ	3,770.0	GJ x 90% x	\$4.503	=	\$15,278.6800	3,770.0	GJ x 90% x	\$7.591	=	\$25,756.2600	\$3.088	10,477.58	25.90%
18	Cost of Biomethane	3,770.0	GJ x 10% x	\$13.808	=	5,205.6200	3,770.0	GJ x 10% x	\$13.808	=	5,205.6200	\$0.000	0.00	0.00%
19	Subtotal Commodity Related Charges per GJ					<b>\$24,921.59</b>					<b>\$35,399.17</b>		<b>\$10,477.58</b>	<b>25.90%</b>
20														
21	Total (with effective \$/GJ rate)	3,770.0		\$10.729		<b>\$40,447.54</b>	3,770.0		\$13.508		<b>\$50,925.12</b>	\$2.779	<b>\$10,477.58</b>	<b>25.90%</b>

Notes: Commodity Cost Recovery Related Charge is based on 90% of the Cost of Gas (Commodity Cost Related Charge) per GJ and 10% of the Cost of Biomethane per GJ.

Notes: Tariff rate schedule per GJ charges are set at 3 decimals. Individual tariff components are calculated and shown to 4 decimals; subtotal amounts, equivalent to the line items on customer bills, are rounded and shown to 2 decimals, consistent with actual invoice calculations.

Slight differences in totals due to rounding



FORTISBC ENERGY INC.  
COMMODITY RELATED CHARGES CHANGES  
BCUC ORDERS G-XX-22

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**RATE SCHEDULE 4 - SEASONAL FIRM GAS SERVICE**

Line No.	Particular	EXISTING RATES JANUARY 1, 2022					TESTED JULY 1, 2022 RATES					Annual Increase/Decrease		
		Quantity		Rate		Annual \$	Quantity		Rate		Annual \$	Rate	Annual \$	% of Previous Total Annual Bill
1														
2	<b>MAINLAND AND VANCOUVER ISLAND SERVICE AREA</b>													
3	<u>Delivery Margin Related Charges</u>													
4	Basic Charge per Day	214	days x	\$14.4230	=	\$3,086.52	214	days x	\$14.4230	=	\$3,086.52	\$0.0000	\$0.00	0.00%
5	Rider 2 Clean Growth Innovation Fund Rate Rider per Day	214	days x	\$0.0131	=	2.80	214	days x	\$0.0131	=	2.80	\$0.000	0.00	0.00%
6	Subtotal of per Day Delivery Margin Related Charges					<b>\$3,089.32</b>					<b>\$3,089.32</b>		<b>\$0.00</b>	<b>0.00%</b>
7														
8	Delivery Charge per GJ													
9	(a) Off-Peak Period	7,960.0	GJ x	\$1.723	=	13,715.0800	7,960.0	GJ x	\$1.723	=	13,715.0800	\$0.000	0.0000	0.00%
10	(b) Extension Period	0.0	GJ x	\$2.368	=	0.0000	0.0	GJ x	\$2.368	=	0.0000	\$0.000	0.0000	0.00%
11	Rider 3 BVA Rate Rider per GJ	7,960.0	GJ x	\$0.059	=	469.6400	7,960.0	GJ x	\$0.059	=	469.6400	\$0.000	0.0000	0.00%
12	Subtotal of Per GJ Delivery Margin Related Charges					<b>\$14,184.72</b>					<b>\$14,184.72</b>		<b>\$0.00</b>	<b>0.00%</b>
13														
14	<u>Commodity Related Charges</u>													
15	Storage and Transport Charge per GJ													
16	(a) Off-Peak Period	7,960.0	GJ x	\$0.912	=	\$7,259.5200	7,960.0	GJ x	\$0.912	=	\$7,259.5200	\$0.000	0.0000	0.00%
17	(b) Extension Period	0.0	GJ x	\$0.912	=	0.0000	0.0	GJ x	\$0.912	=	0.0000	\$0.000	0.0000	0.00%
18	Rider 6 MCRA per GJ	7,960.0	GJ x	(\$0.094 )	=	(748.2400)	7,960.0	GJ x	(\$0.094 )	=	(748.2400)	\$0.000	0.0000	0.00%
19	Commodity Cost Recovery Charge per GJ													
20	(a) Off-Peak Period	7,960.0	GJ x	\$4.503	=	35,843.8800	7,960.0	GJ x	\$7.591	=	60,424.3600	\$3.088	24,580.4800	41.22%
21	(b) Extension Period	0.0	GJ x	\$4.503	=	0.0000	0.0	GJ x	\$7.591	=	0.0000	\$3.088	0.0000	0.00%
22														
23	Subtotal Cost of Gas (Commodity Related Charges) Off-Peak					<b>\$42,355.16</b>					<b>\$66,935.64</b>		<b>\$24,580.48</b>	<b>41.22%</b>
24														
25	Unauthorized Gas Charge During Peak Period (not forecast)													
26														
27	Total during Off-Peak Period	7,960.0				<b>\$59,629.20</b>	7,960.0				<b>\$84,209.68</b>		<b>\$24,580.48</b>	<b>41.22%</b>

Notes: Tariff rate schedule per GJ charges are set at 3 decimals. Individual tariff components are calculated and shown to 4 decimals; subtotal amounts, equivalent to the line items on customer bills, are rounded and shown to 2 decimals, consistent with actual invoice calculations.  
Slight differences in totals due to rounding

FORTISBC ENERGY INC.  
COMMODITY RELATED CHARGES CHANGES  
BCUC ORDERS G-XX-22  
**RATE SCHEDULE 5 -GENERAL FIRM SERVICE**

Line No.	Particular	EXISTING RATES JANUARY 1, 2022					TESTED JULY 1, 2022 RATES					Annual Increase/Decrease		
		Quantity		Rate		Annual \$	Quantity		Rate		Annual \$	Rate	Annual \$	% of Previous Total Annual Bill
1														
2	<b>MAINLAND AND VANCOUVER ISLAND SERVICE AREA</b>													
3	<u>Delivery Margin Related Charges</u>													
4	Basic Charge per Month	12	months	x	\$469.00	= \$5,628.00	12	months	x	\$469.00	= \$5,628.00	\$0.00	\$0.00	<b>0.00%</b>
5	Rider 2 Clean Growth Innovation Fund Rate Rider per Month	12	months	x	\$0.40	= 4.80	12	months	x	\$0.40	= 4.80	\$0.00	0.00	0.00%
6	Subtotal of per Month Delivery Margin Related Charges					<b>\$5,632.80</b>					<b>\$5,632.80</b>		<b>\$0.00</b>	<b>0.00%</b>
7														
8	Demand Charge per Month per GJ of Daily Demand	69.2	GJ	x	\$27.911	= <b>\$23,177.29</b>	69.2	GJ	x	\$27.911	= <b>\$23,177.29</b>	\$0.000	<b>\$0.00</b>	<b>0.00%</b>
9														
10	Delivery Charge per GJ	15,520.0	GJ	x	\$1.000	= \$15,520.0000	15,520.0	GJ	x	\$1.000	= \$15,520.0000	\$0.000	\$0.0000	0.00%
11	Rider 3 BVA Rate Rider per GJ	15,520.0	GJ	x	\$0.059	= 915.6800	15,520.0	GJ	x	\$0.059	= 915.6800	\$0.000	0.0000	0.00%
12	Subtotal of Per GJ Delivery Margin Related Charges					<b>\$16,435.68</b>					<b>\$16,435.68</b>		<b>\$0.00</b>	<b>0.00%</b>
13														
14	<u>Commodity Related Charges</u>													
15	Storage and Transport Charge per GJ	15,520.0	GJ	x	\$0.912	= \$14,154.2400	15,520.0	GJ	x	\$0.912	= \$14,154.2400	\$0.000	\$0.0000	0.00%
16	Rider 6 MCRA per GJ	15,520.0	GJ	x	(\$0.094)	= (1,458.8800)	15,520.0	GJ	x	(\$0.094)	= (1,458.8800)	\$0.000	0.0000	0.00%
17	Commodity Cost Recovery Charge per GJ	15,520.0	GJ	x	\$4.503	= 69,886.5600	15,520.0	GJ	x	\$7.591	= 117,812.3200	\$3.088	47,925.7600	37.49%
18	Subtotal Gas Commodity Cost (Commodity Related Charge)					<b>\$82,581.92</b>					<b>\$130,507.68</b>		<b>\$47,925.76</b>	<b>37.49%</b>
19														
20	Total (with effective \$/GJ rate)	15,520.0			\$8.236	<b>\$127,827.69</b>	15,520.0			\$11.324	<b>\$175,753.45</b>	\$3.088	<b>\$47,925.76</b>	<b>37.49%</b>

Notes: Tariff rate schedule per GJ charges are set at 3 decimals. Individual tariff components are calculated and shown to 4 decimals; subtotal amounts, equivalent to the line items on customer bills, are rounded and shown to 2 decimals, consistent with actual invoice calculations.  
Slight differences in totals due to rounding

FORTISBC ENERGY INC.  
COMMODITY RELATED CHARGES CHANGES  
BCUC ORDERS G-XX-22

RATE SCHEDULE 6 - NATURAL GAS VEHICLE SERVICE

Line No.	Particular	EXISTING RATES JANUARY 1, 2022			TESTED JULY 1, 2022 RATES			Annual Increase/Decrease		
		Quantity	Rate	Annual \$	Quantity	Rate	Annual \$	Rate	Annual \$	% of Previous Annual Bill
1										
2	MAINLAND AND VANCOUVER ISLAND SERVICE AREA									
3	Delivery Margin Related Charges									
4	Basic Charge per Day	365.25	days x \$2.0041	= \$732.00	365.25	days x \$2.0041	= \$732.00	\$0.0000	\$0.00	0.00%
5	Rider 2 Clean Growth Innovation Fund Rate Rider per Day	365.25	days x \$0.0131	= 4.7848	365.25	days x \$0.0131	= 4.7848	\$0.000	0.0000	0.00%
6	Subtotal of per Day Delivery Margin Related Charges			\$736.78			\$736.78		\$0.00	0.00%
7										
8	Delivery Charge per GJ	1,730.0	GJ x \$3.446	= 5,961.5800	1,730.0	GJ x \$3.446	= 5,961.5800	\$0.000	0.0000	0.00%
9	Rider 3 BVA Rate Rider per GJ	1,730.0	GJ x \$0.059	= 102.0700	1,730.0	GJ x \$0.059	= 102.0700	\$0.000	0.0000	0.00%
10	Subtotal of Per GJ Delivery Margin Related Charges			\$6,063.65			\$6,063.65		\$0.00	0.00%
11										
12	Commodity Related Charges									
13	Storage and Transport Charge per GJ	1,730.0	GJ x \$0.470	= \$813.1000	1,730.0	GJ x \$0.470	= \$813.1000	\$0.000	\$0.0000	0.00%
14	Rider 6 MCRA per GJ	1,730.0	GJ x (\$0.048 )	= (83.0400)	1,730.0	GJ x (\$0.048 )	= (83.0400)	\$0.000	0.0000	0.00%
15	Commodity Cost Recovery Charge per GJ	1,730.0	GJ x \$4.503	= 7,790.1900	1,730.0	GJ x \$7.591	= 13,132.4300	\$3.088	5,342.2400	34.87%
16	Subtotal Cost of Gas (Commodity Related Charge)			\$8,520.25			\$13,862.49		\$5,342.24	34.87%
17										
18	Total (with effective \$/GJ rate)	1,730.0	\$8.856	\$15,320.68	1,730.0	\$11.944	\$20,662.92	\$3.088	\$5,342.24	34.87%

Notes: Tariff rate schedule per GJ charges are set at 3 decimals. Individual tariff components are calculated and shown to 4 decimals; subtotal amounts, equivalent to the line items on customer bills, are rounded and shown to 2 decimals, consistent with actual invoice calculations.  
Slight differences in totals due to rounding

FORTISBC ENERGY INC.  
COMMODITY RELATED CHARGES CHANGES  
BCUC ORDERS G-XX-22

**RATE SCHEDULE 7 - GENERAL INTERRUPTIBLE SERVICE**

Line No.	Particular	EXISTING RATES JANUARY 1, 2022					TESTED JULY 1, 2022 RATES					Annual Increase/Decrease		
		Quantity		Rate		Annual \$	Quantity		Rate		Annual \$	Rate	Annual \$	% of Previous Annual Bill
1														
2	<b>MAINLAND AND VANCOUVER ISLAND SERVICE AREA</b>													
3	<u>Delivery Margin Related Charges</u>													
4	Basic Charge per Month	12	months	x	\$880.00	= \$10,560.00	12	months	x	\$880.00	= \$10,560.00	\$0.00	\$0.00	0.00%
5	Rider 2 Clean Growth Innovation Fund Rate Rider per Month	12	months	x	\$0.40	= 4.80	12	months	x	\$0.40	= 4.80	\$0.00	0.00	0.00%
6	Subtotal of per Month Delivery Margin Related Charges					<b>\$10,564.80</b>					<b>\$10,564.80</b>		<b>\$0.00</b>	<b>0.00%</b>
7														
8	Delivery Charge per GJ	140,600.0	GJ	x	\$1.616	= \$227,209.6000	140,600.0	GJ	x	\$1.616	= \$227,209.6000	\$0.000	\$0.0000	0.00%
9	Rider 3 BVA Rate Rider per GJ	140,600.0	GJ	x	\$0.059	= 8,295.4000	140,600.0	GJ	x	\$0.059	= 8,295.4000	\$0.000	0.0000	0.00%
10	Subtotal of Per GJ Delivery Margin Related Charges					<b>\$235,505.00</b>					<b>\$235,505.00</b>		<b>\$0.00</b>	<b>0.00%</b>
11														
12	<u>Commodity Related Charges</u>													
13	Storage and Transport Charge per GJ	140,600.0	GJ	x	\$0.912	= \$128,227.2000	140,600.0	GJ	x	\$0.912	= \$128,227.2000	\$0.000	\$0.0000	0.00%
14	Rider 6 MCRA per GJ	140,600.0	GJ	x	(\$0.094)	= (13,216.4000)	140,600.0	GJ	x	(\$0.094)	= (13,216.4000)	\$0.000	0.0000	0.00%
15	Commodity Cost Recovery Charge per GJ	140,600.0	GJ	x	\$4.503	= 633,121.8000	140,600.0	GJ	x	\$7.591	= 1,067,294.6000	\$3.088	434,172.8000	43.67%
16	Subtotal Cost of Gas (Commodity Related Charge)					<b>\$748,132.60</b>					<b>\$1,182,305.40</b>		<b>\$434,172.80</b>	<b>43.67%</b>
17														
18	Total (with effective \$/GJ rate)	140,600.0			\$7.071	<b>\$994,202.40</b>	140,600.0			\$10.159	<b>\$1,428,375.20</b>	\$3.088	<b>\$434,172.80</b>	<b>43.67%</b>

Notes: Tariff rate schedule per GJ charges are set at 3 decimals. Individual tariff components are calculated and shown to 4 decimals; subtotal amounts, equivalent to the line items on customer bills, are rounded and shown to 2 decimals, consistent with actual invoice calculations.  
Slight differences in totals due to rounding

FORTISBC ENERGY INC.  
COMMODITY RELATED CHARGES CHANGES  
BCUC ORDERS G-367-21 G-354-21  
**RATE SCHEDULE 46 - LNG SERVICE**

Line No.	Particular	EXISTING RATES JANUARY 1, 2022					TESTED JULY 1, 2022 RATES					Annual Increase/Decrease		
		Quantity		Rate		Annual \$	Quantity		Rate		Annual \$	Rate	Annual \$	% of Previous Annual Bill
1														
2	<b>MAINLAND AND VANCOUVER ISLAND SERVICE AREA</b>													
3	<u>Dispensing Service Charges per GJ</u>													
4														
5	LNG Facility Charge per GJ	112,980.0	GJ x	\$4.21	=	\$475,645.8000	112,980.0	GJ x	\$4.21	=	\$475,645.8000	\$0.00	\$0.00	0.00%
6	Electricity Surcharge per GJ	112,980.0	GJ x	\$1.04	=	117,499.2000	112,980.0	GJ x	\$1.04	=	117,499.2000	\$0.000	0.0000	0.00%
7	LNG Spot Charge per GJ	0.0	GJ x	\$5.50	=	0.0000	0.0	GJ x	\$5.50	=	0.0000	\$0.000	0.0000	0.00%
8	Subtotal of Per GJ Delivery Margin Related Charges					<b>\$593,145.00</b>					<b>\$593,145.00</b>		<b>\$0.00</b>	<b>0.00%</b>
9														
10	<u>Commodity Related Charges</u>													
11	Storage and Transport Charge per GJ	112,980.0	GJ x	\$0.912	=	\$103,037.7600	112,980.0	GJ x	\$0.912	=	\$103,037.7600	\$0.000	\$0.0000	0.00%
12	Rider 6 MCRA per GJ	112,980.0	GJ x	(\$0.094)	=	(10,620.1200)	112,980.0	GJ x	(\$0.094)	=	(10,620.1200)	\$0.000	0.0000	0.00%
13	Commodity Cost Recovery Charge per GJ	112,980.0	GJ x	\$4.503	=	508,748.9400	112,980.0	GJ x	\$7.591	=	857,631.1800	\$3.088	348,882.2400	29.21%
14	Subtotal Cost of Gas (Commodity Related Charges)					<b>\$601,166.58</b>					<b>\$950,048.82</b>		<b>\$348,882.24</b>	<b>29.21%</b>
15														
16	Total (with effective \$/GJ rate)	112,980.0		\$10.571		<b>\$1,194,311.58</b>	112,980.0		\$13.659		<b>\$1,543,193.82</b>	\$3.088	<b>\$348,882.24</b>	<b>29.21%</b>

Notes: Tariff rate schedule per GJ charges are set at 3 decimals. Individual tariff components are calculated and shown to 4 decimals; subtotal amounts, equivalent to the line items on customer bills, are rounded and shown to 2 decimals, consistent with actual invoice calculations.  
Slight differences in totals due to rounding

## Alternative A

---

Use a 24-month prospective rate setting period

**FORTISBC ENERGY INC. - MAINLAND AND VANCOUVER ISLAND SERVICE AREA**  
**CCRA RATE CHANGE TRIGGER MECHANISM**  
**FOR THE FORECAST PERIOD JUL 2022 TO JUN 2024**  
**FIVE-DAY AVERAGE FORWARD PRICES - MAY 10, 11, 12, 13, AND 16, 2022**

Alternative A  
Tab 1  
Page 2

Line	Particulars	Pre-Tax (\$Millions)	Forecast Energy (TJ)	Percentage	Unit Cost (\$/GJ)	Reference / Comment
	(1)	(2)	(3)	(4)	(5)	(6)
1	<b><u>CCRA RATE CHANGE TRIGGER RATIO</u></b>					
2	(a)					
3	Projected Deferral Balance at Jul 1, 2022	\$ 138.3				(Tab 1, Page 1, Col.14, Line 14)
4	Forecast Incurred Gas Costs - Jul 2022 to Jun 2024	\$ 1,600.4				(Tab 1, Page 1, Col.14, Line 15 & Line 28)
5	Forecast Recovery Gas Costs at Existing Recovery Rate - Jul 2022 to Jun 2024	\$ 1,325.4				(Tab 1, Page 1, Col.14, Line 16 & Line 29)
6						
7	<b>CCRA =</b> Forecast Recovered Gas Costs (Line 5)	= \$ 1,325.4		= <b>76.2%</b>		
8	<b>Ratio</b> Forecast Incurred Gas Costs (Line 4) + Projected CCRA Balance (Line 3)	\$ 1,738.7				<b>Outside 95% to 105% deadband</b>
9						
10						
11						
12						
13	<b><u>Existing Cost of Gas (Commodity Cost Recovery Rate), effective January 1, 2022</u></b>				<b>\$ 4.503</b>	
14						
15						
16						
17						
18	<b><u>CCRA RATE CHANGE THRESHOLD (+/- \$0.50/GJ)</u></b>					
19						
20	Forecast 24-month CCRA Baseload - Jul 2022 to Jun 2024		294,332			
21						
22	Projected Deferral Balance at Jul 1, 2022 (a)	\$ 138.3			\$ 0.4698 (b)	
23	Forecast 24-month CCRA Activities - Jul 2022 to Jun 2024	\$ 275.1			\$ 0.9345 (b)	
24	(Over) / Under Recovery at Existing Rate	\$ 413.3				(Line 3 + Line 4 - Line 5)
25						
26	<b>Tested Rate (Decrease) / Increase</b>				<b>\$ 1.404 (b)</b>	<b>Exceeds minimum +/- \$0.50/GJ threshold &amp; +/- \$1.00/GJ cap</b>

Notes:

(a) Pre-tax opening balances are restated based on current income tax rates, to reflect grossed-up after tax amounts.

(b) Commodity cost recovery rate in tariff is set at 3 decimal places. Individual rate components are shown to 4 decimal places.

Slight differences in totals due to rounding.

**FORTISBC ENERGY INC. - MAINLAND AND VANCOUVER ISLAND SERVICE AREA**  
**CCRA MONTHLY BALANCES AT PROPOSED RATES BASED ON 24-MONTH AMORTIZATION PERIOD**  
**FOR THE FORECAST PERIOD FROM JUL 2022 TO JUN 2024**  
**FIVE-DAY AVERAGE FORWARD PRICES - May 10, 11, 12, 13, AND 16, 2022**  
**\$(Millions)**

**Alternative A**

Tab 3

Page 1

Line	(1)	(2)	(3)	(4)	(5)	(6)	(7)	(8)	(9)	(10)	(11)	(12)	(13)	(14)
1		Recorded	Recorded	Recorded	Recorded	Projected	Projected							<b>Jan-22</b>
2		Jan-22	Feb-22	Mar-22	Apr-22	May-22	Jun-22							<b>to</b>
														<b>Jun-22</b>
3	CCRA Balance - Beginning (Pre-tax) <sup>(a)</sup>	\$ 68	\$ 68	\$ 71	\$ 76	\$ 89	\$ 107							<b>\$ 68</b>
4	Gas Costs Incurred	55	54	61	67	75	85							396
5	Revenue from <b>APPROVED</b> Recovery Rate	(56)	(51)	(56)	(54)	(56)	(54)							(327)
6	<b>CCRA Balance - Ending (Pre-tax) <sup>(b)</sup></b>	<b>\$ 68</b>	<b>\$ 71</b>	<b>\$ 76</b>	<b>\$ 89</b>	<b>\$ 107</b>	<b>\$ 138</b>							<b>\$ 138</b>
7														
8	Tax Rate	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%							27.0%
9														
10	<b>CCRA Balance - Ending (After-tax) <sup>(c)</sup></b>	<b>\$ 50</b>	<b>\$ 52</b>	<b>\$ 56</b>	<b>\$ 65</b>	<b>\$ 78</b>	<b>\$ 101</b>							<b>\$ 101</b>
11														
12														
13														<b>Jul-22</b>
14		Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	<b>to</b>
15		Jul-22	Aug-22	Sep-22	Oct-22	Nov-22	Dec-22	Jan-23	Feb-23	Mar-23	Apr-23	May-23	Jun-23	<b>Jun-23</b>
16	CCRA Balance - Beginning (Pre-tax) <sup>(a)</sup>	\$ 138	\$ 154	\$ 165	\$ 177	\$ 189	\$ 212	\$ 241	\$ 270	\$ 296	\$ 307	\$ 289	\$ 267	<b>\$ 138</b>
17	Gas Costs Incurred	89	85	83	86	94	102	103	92	84	53	51	50	972
18	Revenue from <b>PROPOSED</b> Recovery Rates	(73)	(73)	(71)	(73)	(71)	(73)	(73)	(66)	(73)	(71)	(73)	(71)	(864)
19	<b>CCRA Balance - Ending (Pre-tax) <sup>(b)</sup></b>	<b>\$ 154</b>	<b>\$ 165</b>	<b>\$ 177</b>	<b>\$ 189</b>	<b>\$ 212</b>	<b>\$ 241</b>	<b>\$ 270</b>	<b>\$ 296</b>	<b>\$ 307</b>	<b>\$ 289</b>	<b>\$ 267</b>	<b>\$ 246</b>	<b>\$ 246</b>
20														
21	Tax Rate	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%
22														
23	<b>CCRA Balance - Ending (After-tax) <sup>(c)</sup></b>	<b>\$ 112</b>	<b>\$ 121</b>	<b>\$ 129</b>	<b>\$ 138</b>	<b>\$ 155</b>	<b>\$ 176</b>	<b>\$ 197</b>	<b>\$ 216</b>	<b>\$ 224</b>	<b>\$ 211</b>	<b>\$ 195</b>	<b>\$ 180</b>	<b>\$ 180</b>
24														
25														
26														<b>Jul-23</b>
27		Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	<b>to</b>
28		Jul-23	Aug-23	Sep-23	Oct-23	Nov-23	Dec-23	Jan-24	Feb-24	Mar-24	Apr-24	May-24	Jun-24	<b>Jun-24</b>
29	CCRA Balance - Beginning (Pre-tax) <sup>(a)</sup>	\$ 246	\$ 224	\$ 203	\$ 180	\$ 157	\$ 142	\$ 130	\$ 119	\$ 109	\$ 92	\$ 64	\$ 32	<b>\$ 246</b>
30	Gas Costs Incurred	52	52	49	51	56	62	64	59	57	43	42	40	628
31	Revenue from <b>PROPOSED</b> Recovery Rates	(74)	(74)	(72)	(74)	(72)	(74)	(74)	(69)	(74)	(72)	(74)	(72)	(874)
32	<b>CCRA Balance - Ending (Pre-tax) <sup>(b)</sup></b>	<b>\$ 224</b>	<b>\$ 203</b>	<b>\$ 180</b>	<b>\$ 157</b>	<b>\$ 142</b>	<b>\$ 130</b>	<b>\$ 119</b>	<b>\$ 109</b>	<b>\$ 92</b>	<b>\$ 64</b>	<b>\$ 32</b>	<b>\$ 0</b>	<b>\$ 0</b>
33														
34	Tax Rate	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%
35														
36	<b>CCRA Balance - Ending (After-tax) <sup>(c)</sup></b>	<b>\$ 164</b>	<b>\$ 148</b>	<b>\$ 131</b>	<b>\$ 115</b>	<b>\$ 103</b>	<b>\$ 95</b>	<b>\$ 87</b>	<b>\$ 80</b>	<b>\$ 67</b>	<b>\$ 46</b>	<b>\$ 23</b>	<b>\$ 0</b>	<b>\$ 0</b>

Notes:

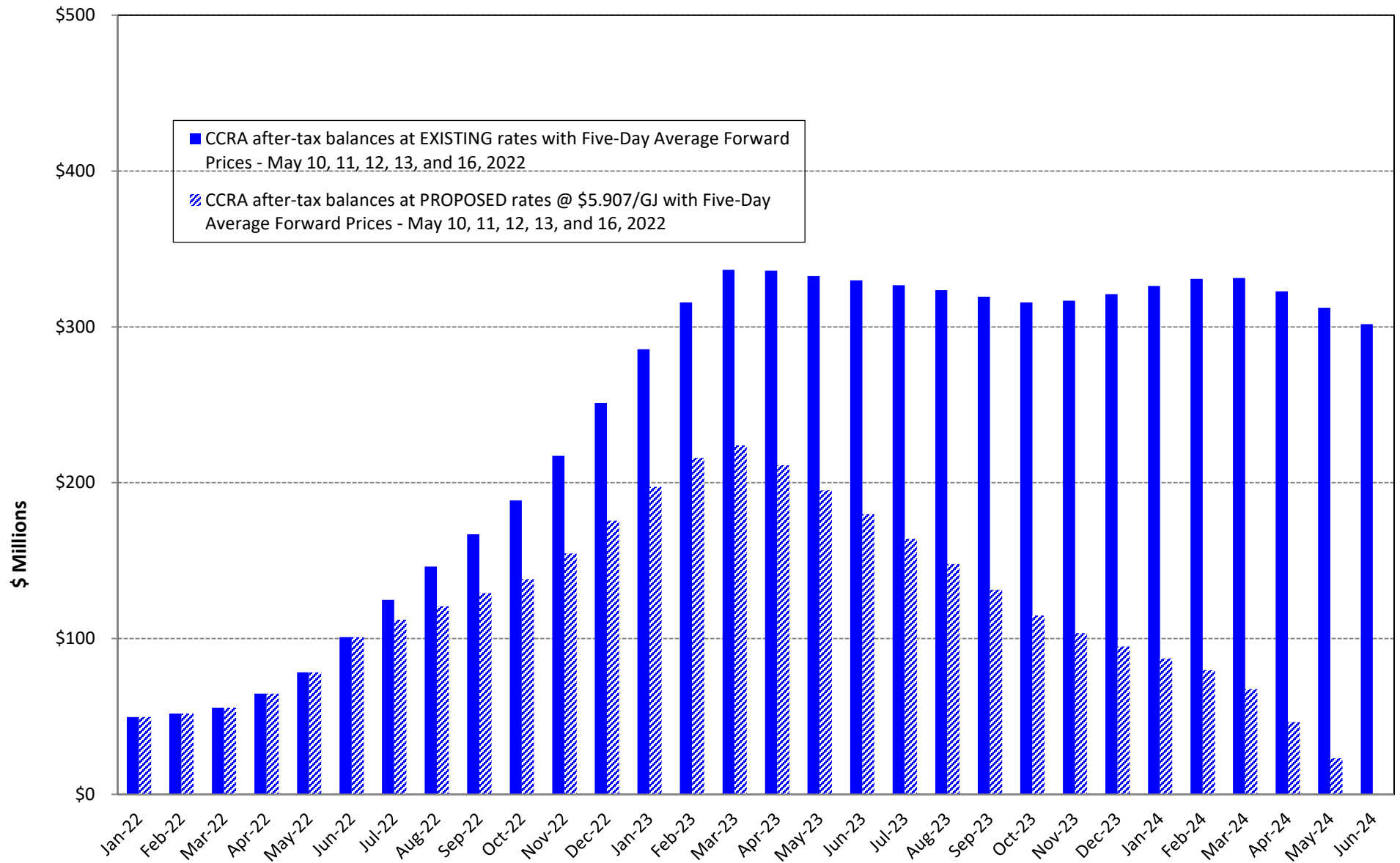
- (a) Pre-tax opening balances are restated based on current income tax rates, to reflect grossed-up after tax amounts.  
(b) For rate setting purposes CCRA pre-tax balances include grossed-up projected deferred interest of approximately \$0.5 million as at June 30, 2022.  
(c) For rate setting purposes CCRA after tax balances are independently grossed-up to reflect pre-tax amounts.

Slight differences in totals due to rounding.



FortisBC Energy Inc. - Mainland and Vancouver Island Service Area  
 CCRA After-Tax Monthly Balances  
 Recorded to April 2022 and Forecast June 2024

Alternative A  
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 Page 2



**FORTISBC ENERGY INC. - MAINLAND AND VANCOUVER ISLAND SERVICE AREA**  
**MCRA MONTHLY BALANCES AT PROPOSED COMMODITY COST RECOVERY CHARGE, STORAGE AND TRANSPORT CHARGES (AFTER ADJUSTMENTS FOR ENERGY DIFFERENCES)**  
**FOR THE FORECAST PERIOD FROM JUL 2022 TO DEC 2024**  
**FIVE-DAY AVERAGE FORWARD PRICES - MAY 10, 11, 12, 13, AND 16, 2022**  
**\$(Millions)**

**Alternative A**  
Tab 3  
Page 3

Line	(1)	(2)	(3)	(4)	(5)	(6)	(7)	(8)	(9)	(10)	(11)	(12)	(13)	(14)
1														
2		Recorded	Recorded	Recorded	Recorded	Projected	Projected	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	<b>Total</b>
3		Jan-22	Feb-22	Mar-22	Apr-22	May-22	Jun-22	Jul-22	Aug-22	Sep-22	Oct-22	Nov-22	Dec-22	<b>2022</b>
4	MCRA Balance - Beginning (Pre-tax) <sup>(a)</sup>	\$ (40)	\$ (58)	\$ (66)	\$ (72)	\$ (80)	\$ (78)	\$ (85)	\$ (83)	\$ (80)	\$ (77)	\$ (75)	\$ (72)	\$ (40)
5	<b>2022 MCRA Activities</b>													
6	Rate Rider 6													
7	Approved Amount to be amortized in 2022	\$ (22)												
8	Rider 6 Amortization at APPROVED 2022 Rates	\$ 3	\$ 3	\$ 2	\$ 2	\$ 1	\$ 1	\$ 1	\$ 1	\$ 1	\$ 2	\$ 3	\$ 4	\$ 22
9	Midstream Base Rates													
10	Gas Costs Incurred	\$ 66	\$ 54	\$ 33	\$ 18	\$ (13)	\$ (28)	\$ (39)	\$ (40)	\$ (28)	\$ 6	\$ 51	\$ 99	\$ 179
11	Revenue from <b>APPROVED</b> Recovery Rates	(86)	(64)	(42)	(28)	14	22	41	42	30	(5)	(51)	(99)	(226)
12	Total Midstream Base Rates (Pre-tax)	\$ (21)	\$ (10)	\$ (9)	\$ (9)	\$ 0	\$ (6)	\$ 1	\$ 3	\$ 3	\$ 0	\$ (0)	\$ 0	\$ (47)
13														
14	<b>MCRA Cumulative Balance - Ending (Pre-tax) <sup>(b)</sup></b>	\$ (58)	\$ (66)	\$ (72)	\$ (80)	\$ (78)	\$ (85)	\$ (83)	\$ (80)	\$ (77)	\$ (75)	\$ (72)	\$ (68)	\$ (68)
15	Tax Rate	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%
16														
17	<b>MCRA Cumulative Balance - Ending (After-tax) <sup>(c)</sup></b>	\$ (42)	\$ (48)	\$ (53)	\$ (58)	\$ (57)	\$ (62)	\$ (61)	\$ (58)	\$ (56)	\$ (54)	\$ (53)	\$ (50)	\$ (50)
18														
19		Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	Forecast	<b>Total</b>
20		Jan-23	Feb-23	Mar-23	Apr-23	May-23	Jun-23	Jul-23	Aug-23	Sep-23	Oct-23	Nov-23	Dec-23	<b>2023</b>
21	MCRA Balance - Beginning (Pre-tax) <sup>(a)</sup>	\$ (68)	\$ (65)	\$ (63)	\$ (61)	\$ (56)	\$ (39)	\$ (18)	\$ 7	\$ 30	\$ 51	\$ 55	\$ 45	\$ (68)
22	<b>2023 MCRA Activities</b>													
23	Rate Rider 6													
24														
25	Rider 6 Amortization at APPROVED 2022 Rates	\$ 3	\$ 3	\$ 2	\$ 2	\$ 1	\$ 1	\$ 1	\$ 1	\$ 1	\$ 2	\$ 3	\$ 4	\$ 23
26	Midstream Base Rates													
27	Gas Costs Incurred	\$ 95	\$ 74	\$ 44	\$ 18	\$ (4)	\$ (10)	\$ (17)	\$ (19)	\$ (11)	\$ 9	\$ 39	\$ 69	\$ 287
28	Revenue from <b>2022</b> Recovery Rates	(96)	(75)	(45)	(15)	21	31	41	42	30	(6)	(52)	(100)	(223)
29	Total Midstream Base Rates (Pre-tax)	\$ (1)	\$ (0)	\$ (1)	\$ 3	\$ 16	\$ 20	\$ 24	\$ 23	\$ 19	\$ 3	\$ (13)	\$ (31)	\$ 63
30														
31	<b>MCRA Cumulative Balance - Ending (Pre-tax) <sup>(b)</sup></b>	\$ (65)	\$ (63)	\$ (61)	\$ (56)	\$ (39)	\$ (18)	\$ 7	\$ 30	\$ 51	\$ 55	\$ 45	\$ 18	\$ 18
32	Tax Rate	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%	27.0%
33														
34	<b>MCRA Cumulative Balance - Ending (After-tax) <sup>(c)</sup></b>	\$ (48)	\$ (46)	\$ (45)	\$ (41)	\$ (28)	\$ (13)	\$ 5	\$ 22	\$ 37	\$ 40	\$ 33	\$ 13	\$ 13

Notes:

- (a) Pre-tax opening balances are restated based on current income tax rates, to reflect grossed-up after tax amounts.  
(b) For rate setting purposes MCRA pre-tax balances include grossed-up projected deferred interest of approximately \$2.3 million credit as at June 30, 2022.  
(c) For rate setting purposes MCRA after-tax balances are independently grossed-up to reflect pre-tax amounts.

Slight differences in totals due to rounding.

FORTISBC ENERGY INC.  
CALCULATION OF CUSTOMERS' RATES AND TARIFF CONTINUITY  
PROPOSED JULY 1, 2022 RATES  
BCUC ORDERS G-XX-22

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PAGE 1  
SCHEDULE 1

RATE SCHEDULE 1: RESIDENTIAL SERVICE		EXISTING RATES JANUARY 1, 2022	COMMODITY RELATED CHARGES CHANGES	PROPOSED JULY 1, 2022 RATES
Line No.	Particulars	Mainland and Vancouver Island	Mainland and Vancouver Island	Mainland and Vancouver Island
	(1)	(2)	(3)	(4)
1	<u>Delivery Margin Related Charges</u>			
2	<b>Basic Charge per Day</b>	<b>\$0.4085</b>	<b>\$0.0000</b>	<b>\$0.4085</b>
3	Rider 2 Clean Growth Innovation Fund Rate Rider per Day	\$0.0131	\$0.000	\$0.0131
4	Subtotal of per Day <b>Delivery Margin Related Charges</b>	<b>\$0.4216</b>	<b>\$0.000</b>	<b>\$0.4216</b>
5				
6				
7	Delivery Charge per GJ	\$5.455	\$0.000	\$5.455
8	Rider 3 BVA Rate Rider per GJ	\$0.059	\$0.000	\$0.059
9	Rider 5 RSAM per GJ	\$0.012	\$0.000	\$0.012
10	Subtotal of Per GJ <b>Delivery Margin Related Charges</b>	<b>\$5.526</b>	<b>\$0.000</b>	<b>\$5.526</b>
11				
12				
13	<u>Commodity Related Charges</u>			
14	Storage and Transport Charge per GJ	\$1.505	\$0.000	\$1.505
15	Rider 6 MCRA per GJ	(\$0.154 )	\$0.000	(\$0.154 )
18	Subtotal <b>Storage and Transport Related Charges per GJ</b>	<b>\$1.351</b>	<b>\$0.000</b>	<b>\$1.351</b>
19				
20	<b>Cost of Gas (Commodity Cost Recovery Charge) per GJ</b>	<b>\$4.503</b>	<b>\$1.404</b>	<b>\$5.907</b>

FORTISBC ENERGY INC.  
CALCULATION OF CUSTOMERS' RATES AND TARIFF CONTINUITY  
PROPOSED JULY 1, 2022 RATES  
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PAGE 2  
SCHEDULE 1B

RATE SCHEDULE 1B: RESIDENTIAL BIOMETHANE SERVICE		EXISTING RATES JANUARY 1, 2022	COMMODITY RELATED CHARGES CHANGES	PROPOSED JULY 1, 2022 RATES
Line No.	Particulars	Mainland and Vancouver Island	Mainland and Vancouver Island	Mainland and Vancouver Island
	(1)	(2)	(3)	(4)
1	<u>Delivery Margin Related Charges</u>			
2	<b>Basic Charge per Day</b>	<b>\$0.4085</b>	<b>\$0.0000</b>	<b>\$0.4085</b>
3	Rider 2 Clean Growth Innovation Fund Rate Rider per Day	\$0.0131	\$0.000	\$0.0131
4	Subtotal of per Day <b>Delivery Margin Related Charges</b>	<b>\$0.4216</b>	<b>\$0.000</b>	<b>\$0.4216</b>
5				
6	Delivery Charge per GJ	\$5.455	\$0.000	\$5.455
7	Rider 3 BVA Rate Rider per GJ	\$0.059	\$0.000	\$0.059
8	Rider 5 RSAM per GJ	\$0.012	\$0.000	\$0.012
9	Subtotal of Per GJ <b>Delivery Margin Related Charges</b>	<b>\$5.526</b>	<b>\$0.000</b>	<b>\$5.526</b>
10				
11				
12	<u>Commodity Related Charges</u>			
13	Storage and Transport Charge per GJ	\$1.505	\$0.000	\$1.505
14	Rider 6 MCRA per GJ	(\$0.154 )	\$0.000	(\$0.154 )
17	Subtotal <b>Storage and Transport Related Charges per GJ</b>	<b>\$1.351</b>	<b>\$0.000</b>	<b>\$1.351</b>
18				
19				
20	<b>Cost of Gas (Commodity Cost Recovery Charge) per GJ</b>	<b>\$4.503</b>	<b>\$1.404</b>	<b>\$5.907</b>
21				
22	<b>Cost of Biomethane per GJ</b>	<b>\$13.808</b>	<b>\$0.000</b>	<b>\$13.808</b>
23	(Biomethane Energy Recovery Charge)			

FORTISBC ENERGY INC.  
CALCULATION OF CUSTOMERS' RATES AND TARIFF CONTINUITY  
PROPOSED JULY 1, 2022 RATES  
BCUC ORDERS G-XX-22

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PAGE 3  
SCHEDULE 2

RATE SCHEDULE 2: SMALL COMMERCIAL SERVICE		EXISTING RATES JANUARY 1, 2022	COMMODITY RELATED CHARGES CHANGES	PROPOSED JULY 1, 2022 RATES
Line No.	Particulars	Mainland and Vancouver Island	Mainland and Vancouver Island	Mainland and Vancouver Island
	(1)	(2)	(3)	(4)
1	<u>Delivery Margin Related Charges</u>			
2	<b>Basic Charge per Day</b>	<b>\$0.9485</b>	<b>\$0.0000</b>	<b>\$0.9485</b>
3	Rider 2 Clean Growth Innovation Fund Rate Rider per Day	\$0.0131	\$0.000	\$0.0131
4	Subtotal of per Day <b>Delivery Margin Related Charges</b>	<b>\$0.9616</b>	<b>\$0.000</b>	<b>\$0.9616</b>
5				
6	Delivery Charge per GJ	\$4.165	\$0.000	\$4.165
7	Rider 3 BVA Rate Rider per GJ	\$0.059	\$0.000	\$0.059
8	Rider 5 RSAM per GJ	\$0.012	\$0.000	\$0.012
9	Subtotal of Per GJ <b>Delivery Margin Related Charges</b>	<b>\$4.236</b>	<b>\$0.000</b>	<b>\$4.236</b>
10				
11				
12	<u>Commodity Related Charges</u>			
13	Storage and Transport Charge per GJ	\$1.542	\$0.000	\$1.542
14	Rider 6 MCRA per GJ	(\$0.158 )	\$0.000	(\$0.158 )
16	Subtotal <b>Storage and Transport Related Charges per GJ</b>	<b>\$1.384</b>	<b>\$0.000</b>	<b>\$1.384</b>
17				
18	<b>Cost of Gas (Commodity Cost Recovery Charge) per GJ</b>	<b>\$4.503</b>	<b>\$1.404</b>	<b>\$5.907</b>

FORTISBC ENERGY INC.  
CALCULATION OF CUSTOMERS' RATES AND TARIFF CONTINUITY  
PROPOSED JULY 1, 2022 RATES  
BCUC ORDERS G-XX-22

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PAGE 4  
SCHEDULE 2B

RATE SCHEDULE 2B: SMALL COMMERCIAL BIOMETHANE SERVICE		EXISTING RATES JANUARY 1, 2022	COMMODITY RELATED CHARGES CHANGES	PROPOSED JULY 1, 2022 RATES
Line No.	Particulars	Mainland and Vancouver Island	Mainland and Vancouver Island	Mainland and Vancouver Island
	(1)	(2)	(3)	(4)
1	<u>Delivery Margin Related Charges</u>			
2	<b>Basic Charge per Day</b>	<b>\$0.9485</b>	<b>\$0.0000</b>	<b>\$0.9485</b>
3	Rider 2 Clean Growth Innovation Fund Rate Rider per Day	\$0.0131	\$0.000	\$0.0131
4	Subtotal of per Day <b>Delivery Margin Related Charges</b>	<b>\$0.9616</b>	<b>\$0.000</b>	<b>\$0.9616</b>
5				
6	Delivery Charge per GJ	\$4.165	\$0.000	\$4.165
7	Rider 3 BVA Rate Rider per GJ	\$0.059	\$0.000	\$0.059
8	Rider 5 RSAM per GJ	\$0.012	\$0.000	\$0.012
9	Subtotal of Per GJ <b>Delivery Margin Related Charges</b>	<b>\$4.236</b>	<b>\$0.000</b>	<b>\$4.236</b>
10				
11				
12	<u>Commodity Related Charges</u>			
13	Storage and Transport Charge per GJ	\$1.542	\$0.000	\$1.542
14	Rider 6 MCRA per GJ	(\$0.158 )	\$0.000	(\$0.158 )
16	Subtotal <b>Storage and Transport Related Charges per GJ</b>	<b>\$1.384</b>	<b>\$0.000</b>	<b>\$1.384</b>
17				
18	<b>Cost of Gas (Commodity Cost Recovery Charge) per GJ</b>	<b>\$4.503</b>	<b>\$1.404</b>	<b>\$5.907</b>
19				
20	<b>Cost of Biomethane per GJ</b>	<b>\$13.808</b>	<b>\$0.000</b>	<b>\$13.808</b>
21	(Biomethane Energy Recovery Charge)			

FORTISBC ENERGY INC.  
CALCULATION OF CUSTOMERS' RATES AND TARIFF CONTINUITY  
PROPOSED JULY 1, 2022 RATES  
BCUC ORDERS G-XX-22

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PAGE 5  
SCHEDULE 3

RATE SCHEDULE 3: LARGE COMMERCIAL SERVICE		EXISTING RATES JANUARY 1, 2022	COMMODITY RELATED CHARGES CHANGES	PROPOSED JULY 1, 2022 RATES
Line No.	Particulars (1)	Mainland and Vancouver Island (2)	Mainland and Vancouver Island (3)	Mainland and Vancouver Island (4)
1	<u>Delivery Margin Related Charges</u>			
2	<b>Basic Charge per Day</b>	<b>\$4.7895</b>	<b>\$0.0000</b>	<b>\$4.7895</b>
3	Rider 2 Clean Growth Innovation Fund Rate Rider per Day	\$0.0131	\$0.000	\$0.0131
4	Subtotal of per Day <b>Delivery Margin Related Charges</b>	<b>\$4.8026</b>	<b>\$0.000</b>	<b>\$4.8026</b>
5				
6	Delivery Charge per GJ	\$3.582	\$0.000	\$3.582
7	Rider 3 BVA Rate Rider per GJ	\$0.059	\$0.000	\$0.059
8	Rider 5 RSAM per GJ	\$0.012	\$0.000	\$0.012
9	Subtotal of Per GJ <b>Delivery Margin Related Charges</b>	<b>\$3.653</b>	<b>\$0.000</b>	<b>\$3.653</b>
10				
11				
12	<u>Commodity Related Charges</u>			
13	Storage and Transport Charge per GJ	\$1.312	\$0.000	\$1.312
14	Rider 6 MCRA per GJ	(\$0.135 )	\$0.000	(\$0.135 )
16	Subtotal <b>Storage and Transport Related Charges per GJ</b>	<b>\$1.177</b>	<b>\$0.000</b>	<b>\$1.177</b>
17				
18	<b>Cost of Gas (Commodity Cost Recovery Charge) per GJ</b>	<b>\$4.503</b>	<b>\$1.404</b>	<b>\$5.907</b>

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SCHEDULE 3B

RATE SCHEDULE 3B: LARGE COMMERCIAL BIOMETHANE SERVICE		EXISTING RATES JANUARY 1, 2022	COMMODITY RELATED CHARGES CHANGES	PROPOSED JULY 1, 2022 RATES
Line No.	Particulars	Mainland and Vancouver Island	Mainland and Vancouver Island	Mainland and Vancouver Island
	(1)	(2)	(3)	(4)
1	<u>Delivery Margin Related Charges</u>			
2	<b>Basic Charge per Day</b>	<b>\$4.7895</b>	<b>\$0.0000</b>	<b>\$4.7895</b>
3	Rider 2 Clean Growth Innovation Fund Rate Rider per Day	\$0.0131	\$0.000	\$0.0131
4	Subtotal of per Day <b>Delivery Margin Related Charges</b>	<b>\$4.8026</b>	<b>\$0.000</b>	<b>\$4.8026</b>
5				
6	Delivery Charge per GJ	\$3.582	\$0.000	\$3.582
7	Rider 3 BVA Rate Rider per GJ	\$0.059	\$0.000	\$0.059
8	Rider 5 RSAM per GJ	\$0.012	\$0.000	\$0.012
9	Subtotal of Per GJ <b>Delivery Margin Related Charges</b>	<b>\$3.653</b>	<b>\$0.000</b>	<b>\$3.653</b>
10				
11				
12	<u>Commodity Related Charges</u>			
13	Storage and Transport Charge per GJ	\$1.312	\$0.000	\$1.312
14	Rider 6 MCRA per GJ	(\$0.135 )	\$0.000	(\$0.135 )
15	Subtotal <b>Storage and Transport Related Charges per GJ</b>	<b>\$1.177</b>	<b>\$0.000</b>	<b>\$1.177</b>
16				
17	<b>Cost of Gas (Commodity Cost Recovery Charge) per GJ</b>	<b>\$4.503</b>	<b>\$1.404</b>	<b>\$5.907</b>
18				
19	<b>Cost of Biomethane per GJ</b>	<b>\$13.808</b>	<b>\$0.000</b>	<b>\$13.808</b>
20	(Biomethane Energy Recovery Charge)			



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SCHEDULE 4

RATE SCHEDULE 4: SEASONAL FIRM GAS SERVICE		EXISTING RATES JANUARY 1, 2022	COMMODITY RELATED CHARGES CHANGES	PROPOSED JULY 1, 2022 RATES
Line No.	Particulars	Mainland and Vancouver Island	Mainland and Vancouver Island	Mainland and Vancouver Island
	(1)	(2)	(3)	(4)
1	<u>Delivery Margin Related Charges</u>			
2	<b>Basic Charge per Day</b>	\$14.4230	\$0.0000	\$14.4230
3	<b>Rider 2 Clean Growth Innovation Fund Rate Rider per Day</b>	\$0.0131	\$0.000	\$0.0131
4	Subtotal of per Day <b>Delivery Margin Related Charges</b>	<b>\$14.4361</b>	<b>\$0.000</b>	<b>\$14.4361</b>
5				
6	<b>Delivery Charge per GJ</b>			
7	(a) Off-Peak Period	\$1.723	\$0.000	\$1.723
8	(b) Extension Period	\$2.368	\$0.000	\$2.368
9				
10	<b>Rider 3 BVA Rate Rider per GJ</b>	\$0.059	\$0.000	\$0.059
11				
12	<u>Commodity Related Charges</u>			
13	<b>Commodity Cost Recovery Charge per GJ</b>			
14	(a) Off-Peak Period	\$4.503	\$1.404	\$5.907
15	(b) Extension Period	\$4.503	\$1.404	\$5.907
16				
17	<b>Storage and Transport Charge per GJ</b>			
18	(a) Off-Peak Period	\$0.912	\$0.000	\$0.912
19	(b) Extension Period	\$0.912	\$0.000	\$0.912
20				
21	<b>Rider 6 MCRA per GJ</b>	(\$0.094 )	\$0.000	(\$0.094 )
22				
23	Subtotal Commodity Related Charges per GJ			
24	(a) Off-Peak Period	\$5.321	\$1.404	\$6.725
25	(b) Extension Period	\$5.321	\$1.404	\$6.725
26				
27				
28				
29	Unauthorized Gas Charge per gigajoule			
30	during peak period			
31				
32				
33	Total Variable Cost per gigajoule between			
34	(a) Off-Peak Period	\$7.103	\$1.404	\$8.507
35	(b) Extension Period	\$7.748	\$1.404	\$9.152

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SCHEDULE 5

RATE SCHEDULE 5 GENERAL FIRM SERVICE		EXISTING RATES JANUARY 1, 2022	COMMODITY RELATED CHARGES CHANGES	PROPOSED JULY 1, 2022 RATES
Line No.	Particulars	Mainland and Vancouver Island	Mainland and Vancouver Island	Mainland and Vancouver Island
	(1)	(2)	(3)	(4)
1	<u>Delivery Margin Related Charges</u>			
2	<b>Basic Charge per Month</b>	\$469.00	\$0.00	\$469.00
3	<b>Rider 2 Clean Growth Innovation Fund Rate Rider per Month</b>	\$0.40	\$0.00	\$0.40
4	Subtotal of per Month <b>Delivery Margin Related Charges</b>	<b>\$469.40</b>	<b>\$0.00</b>	<b>\$469.40</b>
5				
6	<b>Demand Charge per Month per GJ of Daily Demand</b>	\$27.911	\$0.000	\$27.911
7				
8	<b>Delivery Charge per GJ</b>	\$1.000	\$0.000	\$1.000
9				
10	<b>Rider 3 BVA Rate Rider per GJ</b>	\$0.059	\$0.000	\$0.059
11				
12				
13	<u>Commodity Related Charges</u>			
14	<b>Cost of Gas (Commodity Cost Recovery Charge) per GJ</b>	\$4.503	\$1.404	\$5.907
15	<b>Storage and Transport Charge per GJ</b>	\$0.912	\$0.000	\$0.912
16	<b>Rider 6 MCRA per GJ</b>	(\$0.094)	\$0.000	(\$0.094)
17	Subtotal Commodity Related Charges per GJ	<b>\$5.321</b>	<b>\$1.404</b>	<b>\$6.725</b>
18				
19				
20				
21				
22	<b>Total Variable Cost per gigajoule</b>	<b>\$6.380</b>	<b>\$1.404</b>	<b>\$7.784</b>

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SCHEDULE 5B

RATE SCHEDULE 5B: GENERAL FIRM BIOMETHANE SERVICE		EXISTING RATES JANUARY 1, 2022	COMMODITY RELATED CHARGES CHANGES	PROPOSED JULY 1, 2022 RATES
Line No.	Particulars	Mainland and Vancouver Island	Mainland and Vancouver Island	Mainland and Vancouver Island
	(1)	(2)	(3)	(4)
1	<u>Delivery Margin Related Charges</u>			
2	<b>Basic Charge per Month</b>	\$469.00	\$0.00	\$469.00
3	<b>Rider 2 Clean Growth Innovation Fund Rate Rider per Month</b>	\$0.40	\$0.00	\$0.40
4	Subtotal of per Month <b>Delivery Margin Related Charges</b>	<b>\$469.40</b>	<b>\$0.00</b>	<b>\$469.40</b>
5				
6	<b>Demand Charge per GJ</b>	\$27.911	\$0.000	\$27.911
7				
8	<b>Delivery Charge per GJ</b>	\$1.000	\$0.000	\$1.000
9				
10	<b>Rider 3 BVA Rate Rider per GJ</b>	\$0.059	\$0.000	\$0.059
11				
12				
13	<u>Commodity Related Charges</u>			
14	Storage and Transport Charge per GJ	\$0.912	\$0.000	\$0.912
15	Rider 6 MCRA per GJ	(\$0.094)	\$0.000	(\$0.094)
16	Subtotal <b>Storage and Transport Related Charges per GJ</b>	<b>\$0.818</b>	<b>\$0.000</b>	<b>\$0.818</b>
17				
18	<b>Cost of Gas (Commodity Cost Recovery Charge) per GJ</b>	<b>\$4.503</b>	<b>\$1.404</b>	<b>\$5.907</b>
19				
20	<b>Cost of Biomethane per GJ</b>	<b>\$13.808</b>	<b>\$0.000</b>	<b>\$13.808</b>
21	(Biomethane Energy Recovery Charge)			

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SCHEDULE 6

RATE SCHEDULE 6: NATURAL GAS VEHICLE SERVICE		EXISTING RATES JANUARY 1, 2022	COMMODITY RELATED CHARGES CHANGES	PROPOSED JULY 1, 2022 RATES
Line No.	Particulars	Mainland and Vancouver Island	Mainland and Vancouver Island	Mainland and Vancouver Island
	(1)	(2)	(3)	(4)
1	<u>Delivery Margin Related Charges</u>			
2	<b>Basic Charge per Day</b>	\$2.0041	\$0.0000	\$2.0041
3	<b>Rider 2 Clean Growth Innovation Fund Rate Rider per Day</b>	\$0.0131	\$0.000	\$0.0131
4	Subtotal of per Day <b>Delivery Margin Related Charges</b>	<b>\$2.0172</b>	<b>\$0.000</b>	<b>\$2.0172</b>
5				
6	<b>Delivery Charge per GJ</b>	\$3.446	\$0.000	\$3.446
7				
8	<b>Rider 3 BVA Rate Rider per GJ</b>	\$0.059	\$0.000	\$0.059
9				
10				
11	<u>Commodity Related Charges</u>			
12	<b>Cost of Gas (Commodity Cost Recovery Charge) per GJ</b>	\$4.503	\$1.404	\$5.907
13	<b>Storage and Transport Charge per GJ</b>	\$0.470	\$0.000	\$0.470
14	<b>Rider 6 MCRA per GJ</b>	(\$0.048 )	\$0.000	(\$0.048 )
15	Subtotal Commodity Related Charges per GJ	<b>\$4.925</b>	<b>\$1.404</b>	<b>\$6.329</b>
16				
17				
18	Total Variable Cost per gigajoule	<u>\$8.430</u>	<u>\$1.404</u>	<u>\$9.834</u>

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SCHEDULE 6P - Surrey

RATE SCHEDULE 6P: PUBLIC SERVICE - NATURAL GAS VEHICLE REFUELING SERVICE				
Line No.	Particulars	EXISTING RATES JANUARY 1, 2022	COMMODITY RELATED CHARGES CHANGES	PROPOSED JULY 1, 2022 RATES
	(1)	(2)	(3)	(4)
1	<b>Surrey Fueling Station</b>			
2				
3	<b><u>Delivery Margin Related Charges</u></b>			
4	Delivery Charge per GJ	\$3.446	\$0.000	\$3.446
5	Rider 3 BVA Rate Rider per GJ	\$0.059	\$0.000	\$0.059
6	Subtotal of per Gigajoule <b>Delivery Margin Related Charges</b>	<b>\$3.505</b>	<b>\$0.000</b>	<b>\$3.505</b>
7				
8	<b><u>Commodity Related Charges</u></b>			
9	Cost of Gas (Commodity Cost Recovery Charge) per GJ	\$4.503	\$1.404	\$5.907
10	Storage and Transport Charge per GJ	\$0.470	\$0.000	\$0.470
11	Rider 6 MCRA per GJ	(\$0.048 )	\$0.000	(\$0.048 )
12	Subtotal Commodity Related Charges per GJ	<b>\$4.925</b>	<b>\$1.404</b>	<b>\$6.329</b>
13				
14				
15	<b><u>Station Service Related Charges</u></b>			
16	Compression Charge per gigajoule	\$8.441	\$0.000	\$8.441
17	Subtotal of per Gigajoule <b>Station Service Related Charges</b>	<b>\$8.441</b>	<b>\$0.000</b>	<b>\$8.441</b>
18				
19				
20	Total per Gigajoule Rate	<b>\$16.871</b>	<b>\$1.404</b>	<b>\$18.275</b>

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SCHEDULE 6P - 360S

RATE SCHEDULE 6P: PUBLIC SERVICE - NATURAL GAS VEHICLE REFUELING SERVICE		EXISTING RATES JANUARY 1, 2022	COMMODITY RELATED CHARGES CHANGES	PROPOSED JULY 1, 2022 RATES
Line No.	Particulars			
	(1)	(2)	(3)	(4)
1	<sup>1</sup> E360S Fueling Station			
2				
3	<b><u>Delivery Margin Related Charges</u></b>			
4	Delivery Charge per GJ	\$2.301	\$0.000	\$2.301
5	Rider 3 BVA Rate Rider per GJ	\$0.059	\$0.000	\$0.059
6	Subtotal of per Gigajoule <b>Delivery Margin Related Charges</b>	<b>\$2.36</b>	<b>\$0.000</b>	<b>\$2.360</b>
7				
8	<b><u>Commodity Related Charges</u></b>			
9	Cost of Gas (Commodity Cost Recovery Charge) per GJ	\$4.503	\$1.404	\$5.907
10	Storage and Transport Charge per GJ	\$0.912	\$0.000	\$0.912
11	Rider 6 MCRA per GJ	(\$0.094 )	\$0.000	(\$0.094 )
12	Subtotal Commodity Related Charges per GJ	<b>\$5.321</b>	<b>\$1.404</b>	<b>\$6.725</b>
13				
14				
15	<b><u>Station Service Related Charges</u></b>			
16	Capital Rate per gigajoule	\$3.090	\$0.000	\$3.090
17	O&M Rate per gigajoule	\$2.559	\$0.000	\$2.559
18	OH&M per gigajoule	\$0.520	\$0.000	\$0.520
19	Short Term Charge per gigajoule	\$1.000	\$0.000	\$1.000
20	Spot Charge per gigajoule	\$1.000	\$0.000	\$1.000
21	Host Fee per gigajoule	\$2.500	\$0.000	\$2.500
22	Subtotal of per Gigajoule <b>Station Service Related Charges</b>	<b>\$10.669</b>	<b>\$0.000</b>	<b>\$10.669</b>
23				
24				
25	Total per Gigajoule Rate	<b>\$18.350</b>	<b>\$1.404</b>	<b>\$19.754</b>

<sup>1</sup> Pursuant to BCUC Order G-200-20, rates were originally approved on an interim basis effective July 31, 2020.

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SCHEDULE 6P - Annacis

RATE SCHEDULE 6P: PUBLIC SERVICE - NATURAL GAS VEHICLE REFUELING SERVICE				
Line No.	Particulars	EXISTING RATES NOVEMBER 30, 2021	COMMODITY RELATED CHARGES CHANGES	PROPOSED JULY 1, 2022 RATES
	(1)	(2)	(3)	(4)
1	<sup>1</sup> <b>Annacis Fueling Station</b>			
2				
3	<b><u>Delivery Margin Related Charges</u></b>			
4	Delivery Charge per GJ	\$2.369	\$0.000	\$2.369
5	Rider 3 BVA Rate Rider per GJ	\$0.059	\$0.000	\$0.059
6	Subtotal of per Gigajoule <b>Delivery Margin Related Charges</b>	<b>\$2.428</b>	<b>\$0.000</b>	<b>\$2.428</b>
7				
8	<b><u>Commodity Related Charges</u></b>			
9	Cost of Gas (Commodity Cost Recovery Charge) per GJ	\$4.503	\$1.404	\$5.907
10	Storage and Transport Charge per GJ	\$0.912	\$0.000	\$0.912
11	Rider 6 MCRA per GJ	(\$0.094 )	\$0.000	(\$0.094 )
12	Subtotal Commodity Related Charges per GJ	<b>\$5.321</b>	<b>\$1.404</b>	<b>\$6.725</b>
13				
14				
15	<b><u>Station Service Related Charges</u></b>			
16	Capital Rate per gigajoule	\$4.382	\$0.000	\$4.382
17	O&M Rate per gigajoule	\$2.686	\$0.000	\$2.686
18	OH&M per gigajoule	\$0.520	\$0.000	\$0.520
19	Short Term Charge per gigajoule	\$1.000	\$0.000	\$1.000
20	Spot Charge per gigajoule	\$1.000	\$0.000	\$1.000
21	Host Fee per gigajoule	\$2.500	\$0.000	\$2.500
22	Subtotal of per Gigajoule <b>Station Service Related Charges</b>	<b>\$12.088</b>	<b>\$0.000</b>	<b>\$12.088</b>
23				
24				
25	Total per Gigajoule Rate	<b>\$19.837</b>	<b>\$1.404</b>	<b>\$21.241</b>

<sup>1</sup> Pursuant to BCUC Order G-313-21, rates were approved on an interim basis effective November 30, 2021.

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SCHEDULE 6P - GFL

RATE SCHEDULE 6P: PUBLIC SERVICE - NATURAL GAS VEHICLE REFUELING SERVICE				
Line No.	Particulars	EXISTING RATES NOVEMBER 30, 2021	COMMODITY RELATED CHARGES CHANGES	PROPOSED JULY 1, 2022 RATES
	(1)	(2)	(3)	(4)
1	<b>GFL Abbotsford Fueling Station</b>			
2				
3	<b><u>Delivery Margin Related Charges</u></b>			
4	Delivery Charge per GJ	\$2.867	\$0.000	\$2.867
5	Rider 3 BVA Rate Rider per GJ	\$0.059	\$0.000	\$0.059
6	Subtotal of per Gigajoule <b>Delivery Margin Related Charges</b>	<b>\$2.926</b>	<b>\$0.000</b>	<b>\$2.926</b>
7				
8	<b><u>Commodity Related Charges</u></b>			
9	Cost of Gas (Commodity Cost Recovery Charge) per GJ	\$4.503	\$1.404	\$5.907
10	Storage and Transport Charge per GJ	\$0.912	\$0.000	\$0.912
11	Rider 6 MCRA per GJ	(\$0.094 )	\$0.000	(\$0.094 )
12	Subtotal Commodity Related Charges per GJ	<b>\$5.321</b>	<b>\$1.404</b>	<b>\$6.725</b>
13				
14				
15	<b><u>Station Service Related Charges</u></b>			
16	Capital Rate per gigajoule	\$6.656	\$0.000	\$6.656
17	O&M Rate per gigajoule	\$2.014	\$0.000	\$2.014
18	OH&M per gigajoule	\$0.520	\$0.000	\$0.520
19	Short Term Charge per gigajoule	\$1.000	\$0.000	\$1.000
20	Spot Charge per gigajoule	\$1.000	\$0.000	\$1.000
21	Host Fee per gigajoule	\$2.500	\$0.000	\$2.500
22	Subtotal of per Gigajoule <b>Station Service Related Charges</b>	<b>\$13.690</b>	<b>\$0.000</b>	<b>\$13.690</b>
23				
24				
25	<b>Total per Gigajoule Rate</b>	<b>\$21.937</b>	<b>\$1.404</b>	<b>\$23.341</b>

<sup>1</sup> Pursuant to BCUC Order G-330-21, rates were approved on an interim basis effective November 30, 2021.



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SCHEDULE 7

RATE SCHEDULE 7: GENERAL INTERRUPTIBLE SERVICE		EXISTING RATES JANUARY 1, 2022	COMMODITY RELATED CHARGES CHANGES	PROPOSED JULY 1, 2022 RATES
Line No.	Particulars	Mainland and Vancouver Island	Mainland and Vancouver Island	Mainland and Vancouver Island
	(1)	(2)	(3)	(4)
1	<u>Delivery Margin Related Charges</u>			
2	<b>Basic Charge per Month</b>	\$880.00	\$0.00	\$880.00
3	<b>Rider 2 Clean Growth Innovation Fund Rate Rider per Month</b>	\$0.40	\$0.00	\$0.40
4	Subtotal of per Month <b>Delivery Margin Related Charges</b>	<b>\$880.40</b>	<b>\$0.00</b>	<b>\$880.40</b>
5				
6	<b>Delivery Charge per GJ</b>	\$1.616	\$0.000	\$1.616
7				
8	<b>Rider 3 BVA Rate Rider per GJ</b>	\$0.059	\$0.000	\$0.059
9				
10	<u>Commodity Related Charges</u>			
11	<b>Cost of Gas (Commodity Cost Recovery Charge) per GJ</b>	\$4.503	\$1.404	\$5.907
12	<b>Storage and Transport Charge per GJ</b>	\$0.912	\$0.000	\$0.912
13	<b>Rider 6 MCRA per GJ</b>	(\$0.094)	\$0.000	(\$0.094)
14	Subtotal Commodity Related Charges per GJ	<b>\$5.321</b>	<b>\$1.404</b>	<b>\$6.725</b>
15				
16				
17	Total Variable Cost per gigajoule	<b>\$6.996</b>	<b>\$1.404</b>	<b>\$8.400</b>

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SCHEDULE 46.1

RATE SCHEDULE 46: LNG SERVICE		EXISTING RATES JANUARY 1, 2022	COMMODITY RELATED CHARGES CHANGES	PROPOSED JULY 1, 2022 RATES
Line No.	Particulars	Mainland and Vancouver Island	Mainland and Vancouver Island	Mainland and Vancouver Island
	(1)	(2)	(3)	(4)
1	<u>Dispensing Service Charges per GJ</u>			
2	<b>LNG Facility Charge per GJ</b>	\$4.21	\$0.00	\$4.21
3	<b>Electricity Surcharge per GJ</b>	\$1.04	\$0.00	\$1.04
4	<b>LNG Spot Charge per GJ</b>	\$5.50	\$0.00	\$5.50
5				
6				
7	<u>Commodity Related Charges</u>			
8	Storage and Transport Charge per GJ	\$0.912	\$0.000	\$0.912
9	Rider 6 MCRA per GJ	(\$0.094)	\$0.000	(\$0.094)
10	Subtotal Storage and Transport Related Charges per GJ	<b>\$0.818</b>	<b>\$0.000</b>	<b>\$0.818</b>
11				
12	<b>Cost of Gas (Commodity Cost Recovery Charge) per GJ</b>	<b>\$4.503</b>	<b>\$1.404</b>	<b>\$5.907</b>
13				
14	<b>Cost of Biomethane per GJ</b>	<b>\$13.808</b>	<b>\$0.000</b>	<b>\$13.808</b>
15	(Biomethane Energy Recovery Charge)			
16				
17				
18	Total Variable Cost per gigajoule (excluding LNG Spot Charge per GJ)	<u>\$10.571</u>	<u>\$1.404</u>	<u>\$11.975</u>

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**RATE SCHEDULE 1 - RESIDENTIAL SERVICE**

Line No.	Particular	EXISTING RATES JANUARY 1, 2022				PROPOSED JULY 1, 2022 RATES				Annual Increase/Decrease		
		Quantity		Rate	Annual \$	Quantity		Rate	Annual \$	Rate	Annual \$	% of Previous Total Annual Bill
1	<b>MAINLAND AND VANCOUVER ISLAND SERVICE AREA</b>											
2	<u>Delivery Margin Related Charges</u>											
3	Basic Charge per Day	365.25	days x	\$0.4085	= \$149.20	365.25	days x	\$0.4085	= \$149.20	\$0.0000	\$0.0000	0.00%
4	Rider 2 Clean Growth Innovation Fund Rate Rider per Day	365.25	days x	\$0.0131	= 4.78	365.25	days x	\$0.0131	= 4.78	0.00	0.00	0.00%
5	Subtotal of per Day Delivery Margin Related Charges				<b>\$153.98</b>				<b>\$153.98</b>		<b>\$0.00</b>	<b>0.00%</b>
6												
7	Delivery Charge per GJ	90.0	GJ x	\$5.455	= 490.9500	90.0	GJ x	\$5.455	= 490.9500	\$0.000	\$0.0000	0.00%
8	Rider 3 BVA Rate Rider per GJ	90.0	GJ x	\$0.059	= 5.3100	90.0	GJ x	\$0.059	= 5.3100	\$0.000	0.0000	0.00%
9	Rider 5 RSAM per GJ	90.0	GJ x	\$0.012	= 1.0800	90.0	GJ x	\$0.012	= 1.0800	\$0.000	0.0000	0.00%
10	Subtotal of Per GJ Delivery Margin Related Charges				<b>\$497.34</b>				<b>\$497.34</b>		<b>\$0.00</b>	<b>0.00%</b>
11												
12	<u>Commodity Related Charges</u>											
13	Storage and Transport Charge per GJ	90.0	GJ x	\$1.505	= \$135.4500	90.0	GJ x	\$1.505	= \$135.4500	\$0.000	\$0.0000	0.00%
14	Rider 6 MCRA per GJ	90.0	GJ x	(\$0.154 )	= (13.8600)	90.0	GJ x	(\$0.154 )	= (13.8600)	\$0.000	0.0000	0.00%
15	Subtotal Storage and Transport Related Charges per GJ				\$121.59				\$121.59		\$0.00	0.00%
16												
17	Cost of Gas (Commodity Cost Recovery Charge) per GJ	90.0	GJ x	\$4.503	= \$405.27	90.0	GJ x	\$5.907	= \$531.63	\$1.404	\$126.3600	10.73%
18	Subtotal Commodity Related Charges per GJ				<b>\$526.86</b>				<b>\$653.22</b>		<b>\$126.36</b>	<b>10.73%</b>
19												
20	Total (with effective \$/GJ rate)	90.0		\$13.091	<b>\$1,178.18</b>	90.0		\$14.495	<b>\$1,304.54</b>	\$1.404	<b>\$126.36</b>	<b>10.73%</b>

Notes: Tariff rate schedule per GJ charges are set at 3 decimals. Individual tariff components are calculated and shown to 4 decimals; subtotal amounts, equivalent to the line items on customer bills, are rounded and shown to 2 decimals, consistent with actual invoice calculations.  
Slight differences in totals due to rounding

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**RATE SCHEDULE 1B - RESIDENTIAL BIOMETHANE SERVICE**

Line No.	Particular	EXISTING RATES JANUARY 1, 2022				PROPOSED JULY 1, 2022 RATES				Annual Increase/Decrease		
		Quantity		Rate	Annual \$	Quantity		Rate	Annual \$	Rate	Annual \$	% of Previous Total Annual Bill
1	<b>MAINLAND AND VANCOUVER ISLAND SERVICE AREA</b>											
2	<u>Delivery Margin Related Charges</u>											
3	Basic Charge per Day	365.25	days x	\$0.4085	= \$149.20	365.25	days x	\$0.4085	= \$149.20	\$0.0000	\$0.00	0.00%
4	Rider 2 Clean Growth Innovation Fund Rate Rider per Day	365.25	days x	\$0.0131	= 4.78	365.25	days x	\$0.0131	= 4.78	\$0.000	0.00	0.00%
5	Subtotal of per Day Delivery Margin Related Charges				<b>\$153.98</b>				<b>\$153.98</b>		<b>\$0.00</b>	<b>0.00%</b>
6												
7	Delivery Charge per GJ	90.0	GJ x	\$5.455	= 490.9500	90.0	GJ x	\$5.455	= 490.9500	\$0.000	0.0000	0.00%
8	Rider 3 BVA Rate Rider per GJ	90.0	GJ x	\$0.059	= 5.3100	90.0	GJ x	\$0.059	= 5.3100	\$0.000	0.0000	0.00%
9	Rider 5 RSAM per GJ	90.0	GJ x	\$0.012	= 1.0800	90.0	GJ x	\$0.012	= 1.0800	\$0.000	0.0000	0.00%
10	Subtotal of Per GJ Delivery Margin Related Charges				<b>\$497.34</b>				<b>\$497.34</b>		<b>\$0.00</b>	<b>0.00%</b>
11	<u>Commodity Related Charges</u>											
12	Storage and Transport Charge per GJ	90.0	GJ x	\$1.505	= \$135.4500	90.0	GJ x	\$1.505	= \$135.4500	\$0.000	\$0.0000	0.00%
13	Rider 6 MCRA per GJ	90.0	GJ x	(\$0.154 )	= (13.8600)	90.0	GJ x	(\$0.154 )	= (13.8600)	\$0.000	0.0000	0.00%
14	Subtotal Storage and Transport Related Charges per GJ				\$121.59				\$121.59		\$0.00	0.00%
15	Cost of Gas (Commodity Cost Recovery Charge) per GJ	90.0	GJ x 90% x	\$4.503	= 364.74	90.0	GJ x 90% x	\$5.907	= 478.47	\$1.404	113.73	9.01%
16	Cost of Biomethane	90.0	GJ x 10% x	\$13.808	= 124.27	90.0	GJ x 10% x	\$13.808	= 124.27	\$0.000	0.00	0.00%
17	Subtotal Commodity Related Charges				<b>\$610.60</b>				<b>\$724.33</b>		<b>\$113.73</b>	<b>9.01%</b>
18												
19	Total (with effective \$/GJ rate)	90.0		\$14.021	<b>\$1,261.92</b>	90.0		\$15.285	<b>\$1,375.65</b>	\$1.264	<b>\$113.73</b>	<b>9.01%</b>

Notes: Commodity Cost Recovery Related Charge is based on 90% of the Cost of Gas (Commodity Cost Related Charge) per GJ and 10% of the Cost of Biomethane per GJ.  
Notes: Tariff rate schedule per GJ charges are set at 3 decimals. Individual tariff components are calculated and shown to 4 decimals; subtotal amounts, equivalent to the line items on customer bills, are rounded and shown to 2 decimals, consistent with actual invoice calculations.  
Slight differences in totals due to rounding

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**RATE SCHEDULE 2 - SMALL COMMERCIAL SERVICE**

Line No.	Particular	EXISTING RATES JANUARY 1, 2022					PROPOSED JULY 1, 2022 RATES					Annual Increase/Decrease		
		Quantity		Rate		Annual \$	Quantity		Rate		Annual \$	Rate	Annual \$	% of Previous Total Annual Bill
1	<b>MAINLAND AND VANCOUVER ISLAND SERVICE AREA</b>													
2	<u>Delivery Margin Related Charges</u>													
3	Basic Charge per Day	365.25	days x	\$0.9485	=	\$346.44	365.25	days x	\$0.9485	=	\$346.44	\$0.0000	\$0.00	0.00%
4	Rider 2 Clean Growth Innovation Fund Rate Rider per Day	365.25	days x	\$0.0131	=	4.78	365.25	days x	\$0.0131	=	4.78	\$0.000	0.00	0.00%
5	Subtotal of per Day Delivery Margin Related Charges					<b>\$351.22</b>					<b>\$351.22</b>		<b>\$0.00</b>	<b>0.00%</b>
6														
7	Delivery Charge per GJ	340.0	GJ x	\$4.165	=	1,416.1000	340.0	GJ x	\$4.165	=	1,416.1000	\$0.000	0.0000	0.00%
8	Rider 3 BVA Rate Rider per GJ	340.0	GJ x	\$0.059	=	20.0600	340.0	GJ x	\$0.059	=	20.0600	\$0.000	0.0000	0.00%
9	Rider 5 RSAM per GJ	340.0	GJ x	\$0.012	=	4.0800	340.0	GJ x	\$0.012	=	4.0800	\$0.000	0.0000	0.00%
10	Subtotal of Per GJ Delivery Margin Related Charges					<b>\$1,440.24</b>					<b>\$1,440.24</b>		<b>\$0.00</b>	<b>0.00%</b>
11														
12	<u>Commodity Related Charges</u>													
13	Storage and Transport Charge per GJ	340.0	GJ x	\$1.542	=	\$524.2800	340.0	GJ x	\$1.542	=	\$524.2800	\$0.000	\$0.0000	0.00%
14	Rider 6 MCRA per GJ	340.0	GJ x	(\$0.158 )	=	(53.7200)	340.0	GJ x	(\$0.158 )	=	(53.7200)	\$0.000	0.0000	0.00%
15	Subtotal Storage and Transport Related Charges per GJ					\$470.56					\$470.56		\$0.00	0.00%
16														
17	Cost of Gas (Commodity Cost Recovery Charge) per GJ	340.0	GJ x	\$4.503	=	\$1,531.02	340.0	GJ x	\$5.907	=	\$2,008.38	\$1.404	\$477.36	12.59%
18	Subtotal Commodity Related Charges per GJ					<b>\$2,001.58</b>					<b>\$2,478.94</b>		<b>\$477.36</b>	<b>12.59%</b>
19														
20	Total (with effective \$/GJ rate)	340.0		\$11.156		<b>\$3,793.04</b>	340.0		\$12.560		<b>\$4,270.40</b>	\$1.404	<b>\$477.36</b>	<b>12.59%</b>

Notes: Tariff rate schedule per GJ charges are set at 3 decimals. Individual tariff components are calculated and shown to 4 decimals; subtotal amounts, equivalent to the line items on customer bills, are rounded and shown to 2 decimals, consistent with actual invoice calculations.  
Slight differences in totals due to rounding

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**RATE SCHEDULE 2B-SMALL COMMERCIAL BIOMETHANE SERVICE**

Line No.	Particular	EXISTING RATES JANUARY 1, 2022					PROPOSED JULY 1, 2022 RATES					Annual Increase/Decrease		
		Quantity		Rate		Annual \$	Quantity		Rate		Annual \$	Rate	Annual \$	% of Previous Total Annual Bill
1	<b>MAINLAND AND VANCOUVER ISLAND SERVICE AREA</b>													
2	<u>Delivery Margin Related Charges</u>													
3	Basic Charge per Day	365.25	days x	\$0.9485	=	\$346.44	365.25	days x	\$0.9485	=	\$346.44	\$0.0000	\$0.00	0.00%
4	Rider 2 Clean Growth Innovation Fund Rate Rider per Day	365.25	days x	\$0.0131	=	4.78	365.25	days x	\$0.0131	=	4.78	\$0.000	0.0000	0.00%
5	Subtotal of per Day Delivery Margin Related Charges					<b>\$351.22</b>					<b>\$351.22</b>		<b>\$0.00</b>	<b>0.00%</b>
6														
7	Delivery Charge per GJ	340.0	GJ x	\$4.165	=	1,416.1000	340.0	GJ x	\$4.165	=	1,416.1000	\$0.000	0.0000	0.00%
8	Rider 3 BVA Rate Rider per GJ	340.0	GJ x	\$0.059	=	20.0600	340.0	GJ x	\$0.059	=	20.0600	\$0.000	0.0000	0.00%
9	Rider 5 RSAM per GJ	340.0	GJ x	\$0.012	=	4.0800	340.0	GJ x	\$0.012	=	4.0800	\$0.000	0.0000	0.00%
10	Subtotal of Per GJ Delivery Margin Related Charges					<b>\$1,440.24</b>					<b>\$1,440.24</b>		<b>\$0.00</b>	<b>0.00%</b>
11														
12	<u>Commodity Related Charges</u>													
13	Storage and Transport Charge per GJ	340.0	GJ x	\$1.542	=	\$524.2800	340.0	GJ x	\$1.542	=	\$524.2800	\$0.000	\$0.0000	0.00%
14	Rider 6 MCRA per GJ	340.0	GJ x	(\$0.158 )	=	(53.7200)	340.0	GJ x	(\$0.158 )	=	(53.7200)	\$0.000	0.0000	0.00%
16	Subtotal Storage and Transport Related Charges per GJ					\$470.56					\$470.56		\$0.00	0.00%
17	Cost of Gas (Commodity Cost Recovery Charge) per GJ	340.0	GJ x 90% x	\$4.503	=	\$1,377.9200	340.0	GJ x 90% x	\$5.907	=	\$1,807.5400	\$1.404	429.62	10.45%
18	Cost of Biomethane	340.0	GJ x 10% x	\$13.808	=	469.4700	340.0	GJ x 10% x	\$13.808	=	469.4700	\$0.000	0.00	0.00%
19	Subtotal Commodity Related Charges per GJ					<b>\$2,317.95</b>					<b>\$2,747.57</b>		<b>\$429.62</b>	<b>10.45%</b>
20	Total (with effective \$/GJ rate)	340.0		\$12.087		<b>\$4,109.41</b>	340.0		\$13.350		<b>\$4,539.03</b>	\$1.264	<b>\$429.62</b>	<b>10.45%</b>

Notes: Commodity Cost Recovery Related Charge is based on 90% of the Cost of Gas (Commodity Cost Related Charge) per GJ and 10% of the Cost of Biomethane per GJ.

Notes: Tariff rate schedule per GJ charges are set at 3 decimals. Individual tariff components are calculated and shown to 4 decimals; subtotal amounts, equivalent to the line items on customer bills, are rounded and shown to 2 decimals, consistent with actual invoice calculations.

Slight differences in totals due to rounding

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**RATE SCHEDULE 3 - LARGE COMMERCIAL SERVICE**

Line No.	Particular	EXISTING RATES JANUARY 1, 2022					PROPOSED JULY 1, 2022 RATES					Annual Increase/Decrease		
		Quantity		Rate		Annual \$	Quantity		Rate		Annual \$	Rate	Annual \$	% of Previous Total Annual Bill
1	<b>MAINLAND AND VANCOUVER ISLAND SERVICE AREA</b>													
2	<u>Delivery Margin Related Charges</u>													
3	Basic Charge per Day	365.25	days x	\$4.7895	=	\$1,749.36	365.25	days x	\$4.7895	=	\$1,749.36	\$0.0000	\$0.00	0.00%
4	Rider 2 Clean Growth Innovation Fund Rate Rider per Day	365.25	days x	\$0.0131	=	4.78	365.25	days x	\$0.0131	=	4.78	\$0.000	0.00	0.00%
5	Subtotal of per Day Delivery Margin Related Charges					<b>\$1,754.14</b>					<b>\$1,754.14</b>		<b>\$0.00</b>	<b>0.00%</b>
6														
7	Delivery Charge per GJ	3,770.0	GJ x	\$3.582	=	13,504.1400	3,770.0	GJ x	\$3.582	=	13,504.1400	\$0.000	0.0000	0.00%
8	Rider 3 BVA Rate Rider per GJ	3,770.0	GJ x	\$0.059	=	222.4300	3,770.0	GJ x	\$0.059	=	222.4300	\$0.000	0.0000	0.00%
9	Rider 5 RSAM per GJ	3,770.0	GJ x	\$0.012	=	45.2400	3,770.0	GJ x	\$0.012	=	45.2400	\$0.000	0.0000	0.00%
10	Subtotal of Per GJ Delivery Margin Related Charges					<b>\$13,771.81</b>					<b>\$13,771.81</b>		<b>\$0.00</b>	<b>0.00%</b>
11														
12	<u>Commodity Related Charges</u>													
13	Storage and Transport Charge per GJ	3,770.0	GJ x	\$1.312	=	\$4,946.2400	3,770.0	GJ x	\$1.312	=	\$4,946.2400	\$0.000	\$0.0000	0.00%
14	Rider 6 MCRA per GJ	3,770.0	GJ x	(\$0.135 )	=	(508.9500)	3,770.0	GJ x	(\$0.135 )	=	(508.9500)	\$0.000	0.0000	0.00%
15	Subtotal Storage and Transport Related Charges per GJ					\$4,437.29					\$4,437.29		\$0.00	0.00%
16														
17	Cost of Gas (Commodity Cost Recovery Charge) per GJ	3,770.0	GJ x	\$4.503	=	\$16,976.31	3,770.0	GJ x	\$5.907	=	\$22,269.39	\$1.404	\$5,293.08	14.33%
18	Subtotal Commodity Related Charges per GJ					<b>\$21,413.60</b>					<b>\$26,706.68</b>		<b>\$5,293.08</b>	<b>14.33%</b>
19														
20	Total (with effective \$/GJ rate)	3,770.0		\$9.798		<b>\$36,939.55</b>	3,770.0		\$11.202		<b>\$42,232.63</b>	\$1.404	<b>\$5,293.08</b>	<b>14.33%</b>

Notes: Tariff rate schedule per GJ charges are set at 3 decimals. Individual tariff components are calculated and shown to 4 decimals; subtotal amounts, equivalent to the line items on customer bills, are rounded and shown to 2 decimals, consistent with actual invoice calculations.  
Slight differences in totals due to rounding

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**RATE SCHEDULE 3B - LARGE COMMERCIAL BIOMETHANE SERVICE**

Line No.	Particular	EXISTING RATES JANUARY 1, 2022				PROPOSED JULY 1, 2022 RATES				Annual Increase/Decrease		
		Quantity		Rate	Annual \$	Quantity		Rate	Annual \$	Rate	Annual \$	% of Previous Total Annual Bill
1	<b>MAINLAND AND VANCOUVER ISLAND SERVICE AREA</b>											
2	<u>Delivery Margin Related Charges</u>											
3	Basic Charge per Day	365.25	days x	\$4.7895	= \$1,749.36	365.25	days x	\$4.7895	= \$1,749.36	\$0.0000	\$0.00	0.00%
4	Rider 2 Clean Growth Innovation Fund Rate Rider per Day	365.25	days x	\$0.0131	= 4.78	365.25	days x	\$0.0131	= 4.78	\$0.000	0.00	0.00%
5	Subtotal of per Day Delivery Margin Related Charges				<b>\$1,754.14</b>				<b>\$1,754.14</b>		<b>\$0.00</b>	<b>0.00%</b>
6												
7	Delivery Charge per GJ	3,770.0	GJ x	\$3.582	= 13,504.1400	3,770.0	GJ x	\$3.582	= 13,504.1400	\$0.000	0.0000	0.00%
8	Rider 3 BVA Rate Rider per GJ	3,770.0	GJ x	\$0.059	= 222.4300	3,770.0	GJ x	\$0.059	= 222.4300	\$0.000	0.0000	0.00%
9	Rider 5 RSAM per GJ	3,770.0	GJ x	\$0.012	= 45.2400	3,770.0	GJ x	\$0.012	= 45.2400	\$0.000	0.0000	0.00%
10	Subtotal of Per GJ Delivery Margin Related Charges				<b>\$13,771.81</b>				<b>\$13,771.81</b>		<b>\$0.00</b>	<b>0.00%</b>
11												
12	<u>Commodity Related Charges</u>											
13	Storage and Transport Charge per GJ	3,770.0	GJ x	\$1.312	= \$4,946.2400	3,770.0	GJ x	\$1.312	= \$4,946.2400	\$0.000	\$0.0000	0.00%
14	Rider 6 MCRA per GJ	3,770.0	GJ x	(\$0.135 )	= (508.9500)	3,770.0	GJ x	(\$0.135 )	= (508.9500)	\$0.000	0.0000	0.00%
16	Subtotal Storage and Transport Related Charges per GJ				\$4,437.29				\$4,437.29		\$0.00	0.00%
17	Cost of Gas (Commodity Cost Recovery Charge) per GJ	3,770.0	GJ x 90% x	\$4.503	= \$15,278.6800	3,770.0	GJ x 90% x	\$5.907	= \$20,042.4500	\$1.404	4,763.77	11.78%
18	Cost of Biomethane	3,770.0	GJ x 10% x	\$13.808	= 5,205.6200	3,770.0	GJ x 10% x	\$13.808	= 5,205.6200	\$0.000	0.00	0.00%
19	Subtotal Commodity Related Charges per GJ				<b>\$24,921.59</b>				<b>\$29,685.36</b>		<b>\$4,763.77</b>	<b>11.78%</b>
20												
21	Total (with effective \$/GJ rate)	3,770.0		\$10.729	<b>\$40,447.54</b>	3,770.0		\$11.992	<b>\$45,211.31</b>	\$1.264	<b>\$4,763.77</b>	<b>11.78%</b>

Notes: Commodity Cost Recovery Related Charge is based on 90% of the Cost of Gas (Commodity Cost Related Charge) per GJ and 10% of the Cost of Biomethane per GJ.

Notes: Tariff rate schedule per GJ charges are set at 3 decimals. Individual tariff components are calculated and shown to 4 decimals; subtotal amounts, equivalent to the line items on customer bills, are rounded and shown to 2 decimals, consistent with actual invoice calculations.

Slight differences in totals due to rounding



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**RATE SCHEDULE 4 - SEASONAL FIRM GAS SERVICE**

Line No.	Particular	EXISTING RATES JANUARY 1, 2022					PROPOSED JULY 1, 2022 RATES					Annual Increase/Decrease		
		Quantity		Rate	Annual \$		Quantity		Rate	Annual \$		Rate	Annual \$	% of Previous Total Annual Bill
1														
2	<b>MAINLAND AND VANCOUVER ISLAND SERVICE AREA</b>													
3	<u>Delivery Margin Related Charges</u>													
4	Basic Charge per Day	214	days x	\$14.4230	= \$3,086.52		214	days x	\$14.4230	= \$3,086.52		\$0.0000	\$0.00	0.00%
5	Rider 2 Clean Growth Innovation Fund Rate Rider per Day	214	days x	\$0.0131	= 2.80		214	days x	\$0.0131	= 2.80		\$0.000	0.00	0.00%
6	Subtotal of per Day Delivery Margin Related Charges				<b>\$3,089.32</b>					<b>\$3,089.32</b>			<b>\$0.00</b>	<b>0.00%</b>
7														
8	Delivery Charge per GJ													
9	(a) Off-Peak Period	7,960.0	GJ x	\$1.723	= 13,715.0800		7,960.0	GJ x	\$1.723	= 13,715.0800		\$0.000	0.0000	0.00%
10	(b) Extension Period	0.0	GJ x	\$2.368	= 0.0000		0.0	GJ x	\$2.368	= 0.0000		\$0.000	0.0000	0.00%
11	Rider 3 BVA Rate Rider per GJ	7,960.0	GJ x	\$0.059	= 469.6400		7,960.0	GJ x	\$0.059	= 469.6400		\$0.000	0.0000	0.00%
12	Subtotal of Per GJ Delivery Margin Related Charges				<b>\$14,184.72</b>					<b>\$14,184.72</b>			<b>\$0.00</b>	<b>0.00%</b>
13														
14	<u>Commodity Related Charges</u>													
15	Storage and Transport Charge per GJ													
16	(a) Off-Peak Period	7,960.0	GJ x	\$0.912	= \$7,259.5200		7,960.0	GJ x	\$0.912	= \$7,259.5200		\$0.000	0.0000	0.00%
17	(b) Extension Period	0.0	GJ x	\$0.912	= 0.0000		0.0	GJ x	\$0.912	= 0.0000		\$0.000	0.0000	0.00%
18	Rider 6 MCRA per GJ	7,960.0	GJ x	(\$0.094 )	= (748.2400)		7,960.0	GJ x	(\$0.094 )	= (748.2400)		\$0.000	0.0000	0.00%
19	Commodity Cost Recovery Charge per GJ													
20	(a) Off-Peak Period	7,960.0	GJ x	\$4.503	= 35,843.8800		7,960.0	GJ x	\$5.907	= 47,019.7200		\$1.404	11,175.8400	18.74%
21	(b) Extension Period	0.0	GJ x	\$4.503	= 0.0000		0.0	GJ x	\$5.907	= 0.0000		\$1.404	0.0000	0.00%
22														
23	Subtotal Cost of Gas (Commodity Related Charges) Off-Peak				<b>\$42,355.16</b>					<b>\$53,531.00</b>			<b>\$11,175.84</b>	<b>18.74%</b>
24														
25	Unauthorized Gas Charge During Peak Period (not forecast)													
26														
27	Total during Off-Peak Period	7,960.0			<b>\$59,629.20</b>		7,960.0			<b>\$70,805.04</b>			<b>\$11,175.84</b>	<b>18.74%</b>

Notes: Tariff rate schedule per GJ charges are set at 3 decimals. Individual tariff components are calculated and shown to 4 decimals; subtotal amounts, equivalent to the line items on customer bills, are rounded and shown to 2 decimals, consistent with actual invoice calculations.  
Slight differences in totals due to rounding

FORTISBC ENERGY INC.  
COMMODITY RELATED CHARGES CHANGES  
BCUC ORDERS G-XX-22  
**RATE SCHEDULE 5 -GENERAL FIRM SERVICE**

Line No.	Particular	EXISTING RATES JANUARY 1, 2022					PROPOSED JULY 1, 2022 RATES					Annual Increase/Decrease		
		Quantity		Rate		Annual \$	Quantity		Rate		Annual \$	Rate	Annual \$	% of Previous Total Annual Bill
1														
2	<b>MAINLAND AND VANCOUVER ISLAND SERVICE AREA</b>													
3	<u>Delivery Margin Related Charges</u>													
4	Basic Charge per Month	12	months x	\$469.00	=	\$5,628.00	12	months x	\$469.00	=	\$5,628.00	\$0.00	\$0.00	<b>0.00%</b>
5	Rider 2 Clean Growth Innovation Fund Rate Rider per Month	12	months x	\$0.40	=	4.80	12	months x	\$0.40	=	4.80	\$0.00	0.00	0.00%
6	Subtotal of per Month Delivery Margin Related Charges					<b>\$5,632.80</b>					<b>\$5,632.80</b>		<b>\$0.00</b>	<b>0.00%</b>
7														
8	Demand Charge per Month per GJ of Daily Demand	69.2	GJ x	\$27.911	=	<b>\$23,177.29</b>	69.2	GJ x	\$27.911	=	<b>\$23,177.29</b>	\$0.000	<b>\$0.00</b>	<b>0.00%</b>
9														
10	Delivery Charge per GJ	15,520.0	GJ x	\$1.000	=	\$15,520.0000	15,520.0	GJ x	\$1.000	=	\$15,520.0000	\$0.000	\$0.0000	0.00%
11	Rider 3 BVA Rate Rider per GJ	15,520.0	GJ x	\$0.059	=	915.6800	15,520.0	GJ x	\$0.059	=	915.6800	\$0.000	0.0000	0.00%
12	Subtotal of Per GJ Delivery Margin Related Charges					<b>\$16,435.68</b>					<b>\$16,435.68</b>		<b>\$0.00</b>	<b>0.00%</b>
13														
14	<u>Commodity Related Charges</u>													
15	Storage and Transport Charge per GJ	15,520.0	GJ x	\$0.912	=	\$14,154.2400	15,520.0	GJ x	\$0.912	=	\$14,154.2400	\$0.000	\$0.0000	0.00%
16	Rider 6 MCRA per GJ	15,520.0	GJ x	(\$0.094)	=	(1,458.8800)	15,520.0	GJ x	(\$0.094)	=	(1,458.8800)	\$0.000	0.0000	0.00%
17	Commodity Cost Recovery Charge per GJ	15,520.0	GJ x	\$4.503	=	69,886.5600	15,520.0	GJ x	\$5.907	=	91,676.6400	\$1.404	21,790.0800	17.05%
18	Subtotal Gas Commodity Cost (Commodity Related Charge)					<b>\$82,581.92</b>					<b>\$104,372.00</b>		<b>\$21,790.08</b>	<b>17.05%</b>
19														
20	Total (with effective \$/GJ rate)	15,520.0		\$8.236		<b>\$127,827.69</b>	15,520.0		\$9.640		<b>\$149,617.77</b>	\$1.404	<b>\$21,790.08</b>	<b>17.05%</b>

Notes: Tariff rate schedule per GJ charges are set at 3 decimals. Individual tariff components are calculated and shown to 4 decimals; subtotal amounts, equivalent to the line items on customer bills, are rounded and shown to 2 decimals, consistent with actual invoice calculations.  
Slight differences in totals due to rounding

FORTISBC ENERGY INC.  
COMMODITY RELATED CHARGES CHANGES  
BCUC ORDERS G-XX-22

RATE SCHEDULE 6 - NATURAL GAS VEHICLE SERVICE

Line No.	Particular	EXISTING RATES JANUARY 1, 2022					PROPOSED JULY 1, 2022 RATES					Annual Increase/Decrease		
		Quantity		Rate		Annual \$	Quantity		Rate		Annual \$	Rate	Annual \$	% of Previous Annual Bill
1														
2	MAINLAND AND VANCOUVER ISLAND SERVICE AREA													
3	Delivery Margin Related Charges													
4	Basic Charge per Day	365.25	days x	\$2.0041	=	\$732.00	365.25	days x	\$2.0041	=	\$732.00	\$0.0000	\$0.00	0.00%
5	Rider 2 Clean Growth Innovation Fund Rate Rider per Day	365.25	days x	\$0.0131	=	4.7848	365.25	days x	\$0.0131	=	4.7848	\$0.000	0.0000	0.00%
6	Subtotal of per Day Delivery Margin Related Charges					\$736.78					\$736.78		\$0.00	0.00%
7														
8	Delivery Charge per GJ	1,730.0	GJ x	\$3.446	=	5,961.5800	1,730.0	GJ x	\$3.446	=	5,961.5800	\$0.000	0.0000	0.00%
9	Rider 3 BVA Rate Rider per GJ	1,730.0	GJ x	\$0.059	=	102.0700	1,730.0	GJ x	\$0.059	=	102.0700	\$0.000	0.0000	0.00%
10	Subtotal of Per GJ Delivery Margin Related Charges					\$6,063.65					\$6,063.65		\$0.00	0.00%
11														
12	Commodity Related Charges													
13	Storage and Transport Charge per GJ	1,730.0	GJ x	\$0.470	=	\$813.1000	1,730.0	GJ x	\$0.470	=	\$813.1000	\$0.000	\$0.0000	0.00%
14	Rider 6 MCRA per GJ	1,730.0	GJ x	(\$0.048 )	=	(83.0400)	1,730.0	GJ x	(\$0.048 )	=	(83.0400)	\$0.000	0.0000	0.00%
15	Commodity Cost Recovery Charge per GJ	1,730.0	GJ x	\$4.503	=	7,790.1900	1,730.0	GJ x	\$5.907	=	10,219.1100	\$1.404	2,428.9200	15.85%
16	Subtotal Cost of Gas (Commodity Related Charge)					\$8,520.25					\$10,949.17		\$2,428.92	15.85%
17														
18	Total (with effective \$/GJ rate)	1,730.0		\$8.856		\$15,320.68	1,730.0		\$10.260		\$17,749.60	\$1.404	\$2,428.92	15.85%

Notes: Tariff rate schedule per GJ charges are set at 3 decimals. Individual tariff components are calculated and shown to 4 decimals; subtotal amounts, equivalent to the line items on customer bills, are rounded and shown to 2 decimals, consistent with actual invoice calculations.  
Slight differences in totals due to rounding

FORTISBC ENERGY INC.  
COMMODITY RELATED CHARGES CHANGES  
BCUC ORDERS G-XX-22

**RATE SCHEDULE 7 - GENERAL INTERRUPTIBLE SERVICE**

Line No.	Particular	EXISTING RATES JANUARY 1, 2022					PROPOSED JULY 1, 2022 RATES					Annual Increase/Decrease		
		Quantity		Rate	Annual \$		Quantity		Rate	Annual \$		Rate	Annual \$	% of Previous Annual Bill
1														
2	<b>MAINLAND AND VANCOUVER ISLAND SERVICE AREA</b>													
3	<u>Delivery Margin Related Charges</u>													
4	Basic Charge per Month	12	months x	\$880.00	= \$10,560.00		12	months x	\$880.00	= \$10,560.00		\$0.00	\$0.00	0.00%
5	Rider 2 Clean Growth Innovation Fund Rate Rider per Month	12	months x	\$0.40	= 4.80		12	months x	\$0.40	= 4.80		\$0.00	0.00	0.00%
6	Subtotal of per Month Delivery Margin Related Charges				<b>\$10,564.80</b>					<b>\$10,564.80</b>			<b>\$0.00</b>	<b>0.00%</b>
7														
8	Delivery Charge per GJ	140,600.0	GJ x	\$1.616	= \$227,209.6000		140,600.0	GJ x	\$1.616	= \$227,209.6000		\$0.000	\$0.0000	0.00%
9	Rider 3 BVA Rate Rider per GJ	140,600.0	GJ x	\$0.059	= 8,295.4000		140,600.0	GJ x	\$0.059	= 8,295.4000		\$0.000	0.0000	0.00%
10	Subtotal of Per GJ Delivery Margin Related Charges				<b>\$235,505.00</b>					<b>\$235,505.00</b>			<b>\$0.00</b>	<b>0.00%</b>
11														
12	<u>Commodity Related Charges</u>													
13	Storage and Transport Charge per GJ	140,600.0	GJ x	\$0.912	= \$128,227.2000		140,600.0	GJ x	\$0.912	= \$128,227.2000		\$0.000	\$0.0000	0.00%
14	Rider 6 MCRA per GJ	140,600.0	GJ x	(\$0.094)	= (13,216.4000)		140,600.0	GJ x	(\$0.094)	= (13,216.4000)		\$0.000	0.0000	0.00%
15	Commodity Cost Recovery Charge per GJ	140,600.0	GJ x	\$4.503	= 633,121.8000		140,600.0	GJ x	\$5.907	= 830,524.2000		\$1.404	197,402.4000	19.86%
16	Subtotal Cost of Gas (Commodity Related Charge)				<b>\$748,132.60</b>					<b>\$945,535.00</b>			<b>\$197,402.40</b>	<b>19.86%</b>
17														
18	Total (with effective \$/GJ rate)	140,600.0		\$7.071	<b>\$994,202.40</b>		140,600.0		\$8.475	<b>\$1,191,604.80</b>		\$1.404	<b>\$197,402.40</b>	<b>19.86%</b>

Notes: Tariff rate schedule per GJ charges are set at 3 decimals. Individual tariff components are calculated and shown to 4 decimals; subtotal amounts, equivalent to the line items on customer bills, are rounded and shown to 2 decimals, consistent with actual invoice calculations.  
Slight differences in totals due to rounding

FORTISBC ENERGY INC.  
COMMODITY RELATED CHARGES CHANGES  
BCUC ORDERS G-367-21 G-354-21  
**RATE SCHEDULE 46 - LNG SERVICE**

Line No.	Particular	EXISTING RATES JANUARY 1, 2022					PROPOSED JULY 1, 2022 RATES					Annual Increase/Decrease		
		Quantity		Rate		Annual \$	Quantity		Rate		Annual \$	Rate	Annual \$	% of Previous Annual Bill
1														
2	<b>MAINLAND AND VANCOUVER ISLAND SERVICE AREA</b>													
3	<u>Dispensing Service Charges per GJ</u>													
4														
5	LNG Facility Charge per GJ	112,980.0	GJ x	\$4.21	=	\$475,645.8000	112,980.0	GJ x	\$4.21	=	\$475,645.8000	\$0.00	\$0.00	0.00%
6	Electricity Surcharge per GJ	112,980.0	GJ x	\$1.04	=	117,499.2000	112,980.0	GJ x	\$1.04	=	117,499.2000	\$0.000	0.0000	0.00%
7	LNG Spot Charge per GJ	0.0	GJ x	\$5.50	=	0.0000	0.0	GJ x	\$5.50	=	0.0000	\$0.000	0.0000	0.00%
8	Subtotal of Per GJ Delivery Margin Related Charges					<b>\$593,145.00</b>					<b>\$593,145.00</b>		<b>\$0.00</b>	<b>0.00%</b>
9														
10	<u>Commodity Related Charges</u>													
11	Storage and Transport Charge per GJ	112,980.0	GJ x	\$0.912	=	\$103,037.7600	112,980.0	GJ x	\$0.912	=	\$103,037.7600	\$0.000	\$0.0000	0.00%
12	Rider 6 MCRA per GJ	112,980.0	GJ x	(\$0.094)	=	(10,620.1200)	112,980.0	GJ x	(\$0.094)	=	(10,620.1200)	\$0.000	0.0000	0.00%
13	Commodity Cost Recovery Charge per GJ	112,980.0	GJ x	\$4.503	=	508,748.9400	112,980.0	GJ x	\$5.907	=	667,372.8600	\$1.404	158,623.9200	13.28%
14	Subtotal Cost of Gas (Commodity Related Charges)					<b>\$601,166.58</b>					<b>\$759,790.50</b>		<b>\$158,623.92</b>	<b>13.28%</b>
15														
16	Total (with effective \$/GJ rate)	112,980.0		\$10.571		<b>\$1,194,311.58</b>	112,980.0		\$11.975		<b>\$1,352,935.50</b>	\$1.404	<b>\$158,623.92</b>	<b>13.28%</b>

Notes: Tariff rate schedule per GJ charges are set at 3 decimals. Individual tariff components are calculated and shown to 4 decimals; subtotal amounts, equivalent to the line items on customer bills, are rounded and shown to 2 decimals, consistent with actual invoice calculations.  
Slight differences in totals due to rounding

**ORDER NUMBER**

**G-xx-xx**

**IN THE MATTER OF**

the *Utilities Commission Act*, RSBC 1996, Chapter 473

and

FortisBC Energy Inc.

Application regarding its Second Quarter Gas Cost Report

And Rate Changes effective July 1, 2022

For the Mainland and Vancouver Island Service Area

**BEFORE:**

[Panel Chair]

Commissioner

Commissioner

on Date

**ORDER**

**WHEREAS:**

- A. On June 1, 2022, FortisBC Energy Inc. (FEI) filed its 2022 Second Quarter Gas Cost Report on the Commodity Cost Reconciliation Account (CCRA), Midstream Cost Reconciliation Account (MCRA), and Commodity Rate Effective July 1, 2022 for the Mainland and Vancouver Island Service Area based on the five-day average May 10, 11, 12, 13, and 16, 2022 forward gas prices (the Report);
- B. The BCUC established guidelines for gas cost rate setting in Letter L-5-01 dated February 5, 2001, and further modified the guidelines in Letter L-40-11 dated May 19, 2011 and Letter L-15-16 dated June 16, 2015 (together the Guidelines);
- C. By Order G-354-21, the BCUC approved the current Commodity Cost Recovery Charge for the Mainland and Vancouver Island service area at \$4.503 per gigajoule (\$/GJ) effective January 1, 2022;
- D. In the Report, using the five-day average forward prices ending May 16, 2022, the CCRA balance is projected to be approximately \$138 million deficit after tax as of June 30, 2022. FEI calculates the CCRA recovery-to-cost ratio at the existing rate would be 59.3 percent for the following 12 months, and the rate increase related to the forecast under recovery of gas costs would be \$3.088/GJ which falls outside the minimum rate change thresholds set out in the Guidelines;
- E. In the Report, FEI indicates that the current circumstances fully meet the approved Guidelines criteria when consideration may be given to using a 24-month rate setting period and provides an alternative based on using a 24-month prospective rate setting period. The 24-month rate setting alternative results in a proposed rate increase of \$1.404/GJ to the Commodity Cost Recovery Charge effective July 1, 2022. The

total annual bill for a typical Mainland and Vancouver Island residential customer with an average annual consumption of 90 gigajoules will increase by approximately \$126 or 10.7 percent;

- F. The BCUC reviewed the Report and considers that the requested rate changes are warranted, and that Tab 4 of the Report should be held confidential as requested by FEI, as they contain market sensitive information.

**NOW THEREFORE** pursuant to section 61(4) of the *Utilities Commission Act*, the British Columbia Utilities Commission orders as follows:

1. The Commodity Cost Recovery Charge applicable to the Sales Rate Classes and Rate Schedule 46 LNG Service within the Mainland and Vancouver Island Service Area to increase by \$1.404/GJ, from \$4.503/GJ to \$5.907/GJ, effective July 1, 2022.
2. FEI will notify all customers that are affected by the rate changes with a bill insert or bill message to be included with the next monthly gas billing.
3. The BCUC will hold confidential the information in Tab 4 of the Report, as requested by FEI, as it contains market sensitive information.
4. FEI is directed to file revised tariff pages with the BCUC within 15 days of this order.

**DATED** at the City of Vancouver, in the Province of British Columbia, this (XX) day of (Month) 2020.

BY ORDER

(X. X. last name)  
Commissioner

Attachment